

Development Services

Planning Division
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DOWNTOWN OXNARD PARKLET PILOT PROGRAM

PARKLET DESIGN GUIDELINES

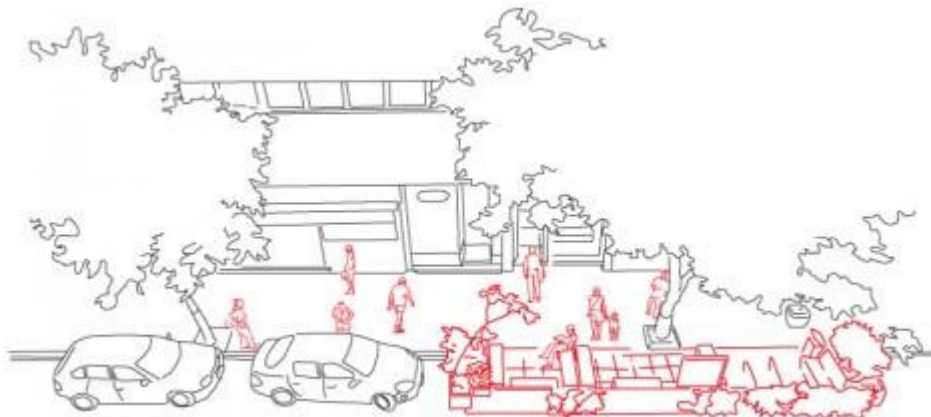


I. INTRODUCTION

As part of the Oxnard Downtown Vision Plan implementation, the City of Oxnard has established a pilot program for the development and operation of Parklets in the Downtown (CBD) area. This program will encourage activation of public spaces, provide opportunities for existing and new restaurants and stimulate outdoor dining experience.

Pursuant to Ordinance No. 2936, the following requirements shall govern the design, permitting, and operation of parklets within the Central Business District (CBD) zone. These requirements may be modified by the Planning Division as needed to ensure compliance with applicable codes and safety regulations.

II. PARKLET LOCATION CRITERIA



- **General Requirements** - Design and location of parklets shall be considered on a case-by-case basis. Factors including line-of-sight, occupied public parking

areas, existing utilities, traffic speeds, etc. may result in additional design requirements by City Staff.

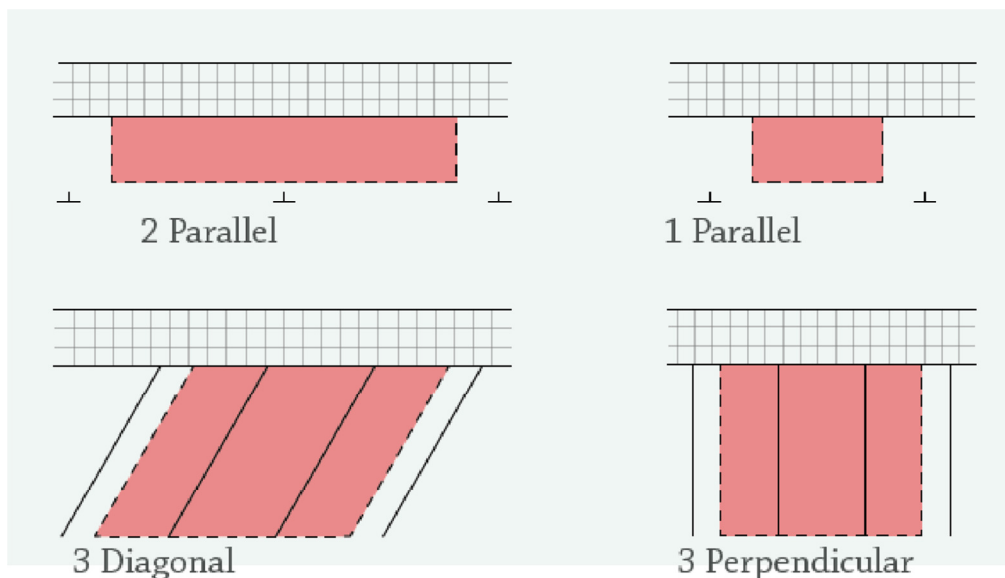
- **Adjacent to an existing or proposed restaurant** - Parklet are permitted when located adjacent to or in front of an existing or proposed restaurant/food service use. The parklet will serve provide an outdoor dining opportunity for patrons of the restaurant. Similarly, the restaurant operator will be responsible for the maintenance and operation of the parklet.
- **Speed limit** - Parklets are permitted on streets with speed limits of 25 mph or less. Parklets on streets with speed limits over 25 mph may be considered on a case-by-case basis.
- **Parking spaces** - Parklets are sited along the curb line on streets where on-street parking spaces exist. They can be considered in any location where there are or would be space(s) for on-street parallel, angled, or perpendicular parking.
- **Corner locations** - In general, parklets should be located at least one parking space away from an intersection or street corner. In some instances, a curb-extension (bulb-out), or some other physical barrier may allow the city to consider a parklet closer to the corner.
- **Fronting Driveways** - Parklets may be installed in front of a driveway if the applicant owns the property served by driveway, or obtains written permission from the property owner. If the driveway has been abandoned or no longer provides access to off-street parking, the driveway may be levelled as part parklet project.
- **Street slope** - Parklets are generally permitted on streets with a running slope (grade) of five percent or less.
- **Curb color zones** - Curb color zones, i.e. disabled access, temporary parking, etc. shall be avoided. Parklets proposed to be located at curb color zones shall be reviewed on a case by case basis by City Staff and may not be granted.
- **Transit** - Parklets are not permitted in bus zones. They may be located adjacent to a bus zone if adequate clearances are provided, as determined by City Staff.
- **Utilities** - Parklets may not be constructed over utility access panels, manhole covers, storm drains, or fire hydrant shut-off valves. Applicants must provide access to any City or public utility company that may have underground conduits

beneath the constructed parklet. Access may require that a parklet sponsor temporarily remove all or a portion the constructed parklet.

III. PARKLET DESIGN CRITERIA

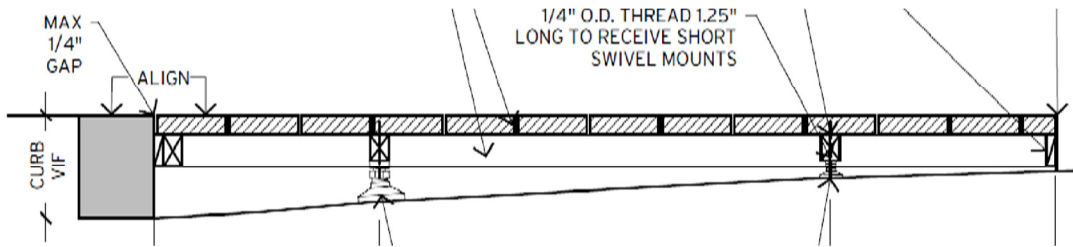


- **Parallel parking** - Parklets can range in length from one to two parking spaces. City Staff will consider larger parklets depending on circumstance and existing site conditions. For parallel parking, the parklet structure must be set back a minimum of 36" up to 48" from adjacent parking spaces.
- **Perpendicular and diagonal parking** - Where parklets are installed in diagonal or perpendicular parking spaces, they are typically designed to be the size of three or more combined parking spaces to maximize habitable space within the parklet. For diagonal and perpendicular spaces, the edge of the parklet must be set back a minimum of 36" and up to 48" from the adjacent parking space on either side.



- **Nearby driveways** - Parklets located next to driveways must be set back 24” from the outside edge of the driveway. If the parklet is proposed in front of a driveway apron, the apron must be filled or a level platform installed to make the area flush with the sidewalk and parklet deck.
- **Lane of Travel** - Parklets shall maintain a 24” setback from the adjacent lane(s) of travel.
- **Traffic Safety Devices** - Parklet design may be required to include features that control traffic and protect the parklet structure. These features will be identified by Staff on a case-by-case basis during preliminary review.
- **No Advertising** - Logos, advertising, or other signage is prohibited within the parklet.
- **Include Parklet Sign** - Applicants shall install a standard “Parklet reserved for customer use only” sign at a visible location within the parklet.
- **Design for easy removal and restoration.** Parklets may sit above of buried infrastructure and utilities such as gas lines, sewer and water mains. Parklets should be designed for easy removal in case of emergency. No parklet component may weigh more than 200 pounds per square foot.

IV. PARKLET PLATFORM



- **Threshold** - Parklets should be designed as an extension of the sidewalk, with continuous access along the curbside edge. Any openings between the sidewalk and the Deck Surface shall be flush without a horizontal or vertical separation greater than 1/2 inch. Changes in level 1/4 inch to 1/2 inch high maximum shall be beveled with a slope not steeper than 1:4 (25%). Where the parklet fronts existing driveways or curb ramps, the driveway area or curb ramp shall be temporarily levelled for the duration of the Parklet's installation.
- **Bolting** - Bolting into the street or penetrating the surface of the road in any way is strongly discouraged. Parklets may be bolted to the existing curb, but only with a restoration plan and performance bond posted by the applicant.
- **Platform surface** - The top of the parklet platform must be flush with the sidewalk with a maximum gap of 1/2 inch.
- **Concrete** - If using a concrete base for the parklet deck, the concrete cannot be poured directly on the road surface. A plastic slip-sheet can be used to prevent the concrete from binding to the roadbed below. To facilitate easy removal of the parklet, the concrete floor should not include structural rebar and must weigh less than 200 pounds per square foot.
- **Surface materials** - Loose particles, such as sand or loose stone, are not permitted on the parklet.
- **Access** - If the platform base is not a solid mass, the clear space underneath the platform surface must be accessible for maintenance through access panels, removable pavers, etc.

- **Drainage** - The parklet cannot impede the flow of curbside drainage. Openings at either end of the parklet shall be covered with screens to prevent debris build-up beneath the deck and in the gutter.

V. PARKLET ENCLOSURE



- **Buffer the edges** - Depending on the location, the parklet should have an edge as a buffer from the street. This can take the form of planters, railing, cabling, or some other appropriate enclosure. The height and scale of the buffer required will vary depending on local context. If cable railing is used, spacing between cables cannot exceed 5 inches.
- **Open Length / Maintain a visual connection to the street** - Designs should allow pedestrians on either side of the street see into the parklet. Continuous opaque walls above forty-two inches along the length of the parklet that do not block views into the parklet from the surrounding streetscape are highly discouraged.
- **Avoid overhead elements that span the sidewalk** - Overhead elements that span the sidewalk and connect the parklet to the adjacent building façade are strongly discouraged. Such proposals may be considered on a case-by-case basis, and will require a minimum vertical clearance of 80 inches above grade.
- **Enclosure Height** - Parklet enclosure shall not exceed 42” as measured from the street side. The Parklet sides adjacent to parking shall have a solid surface and can incorporate planters, low walls, or a combination of both. The street

sides shall be of solid material up to 24” high, and “see through” between 24” and 42” high, as measured from the street level, and shall use planters, cabling, low walls, or a combination of all.

VI. AMENITIES

Lighting



Parklet lighting is encouraged, and is necessary for parklets being used after sunset. Lighting should be either battery or solar powered.

- **Lighting** - Lighting elements are strongly encouraged. Applicants interested in lighting should consider solar-powered lighting or other non-hardwired installations.

Furniture



Movable tables, chairs and benches that are designed to withstand exposure to sun and weather shall be used. No fixed furniture or plastic furniture is allowed.

- **Movable furniture** - Movable tables, chairs and benches that are designed to withstand exposure to sun and weather shall be used. No fixed furniture is allowed.

Planters



Landscaping such as low maintenance and water tolerant plants are highly encouraged.

- **Planting** - Integrated planting is strongly encouraged. Native plants and drought tolerant plants are encouraged.

Bicycle Parking

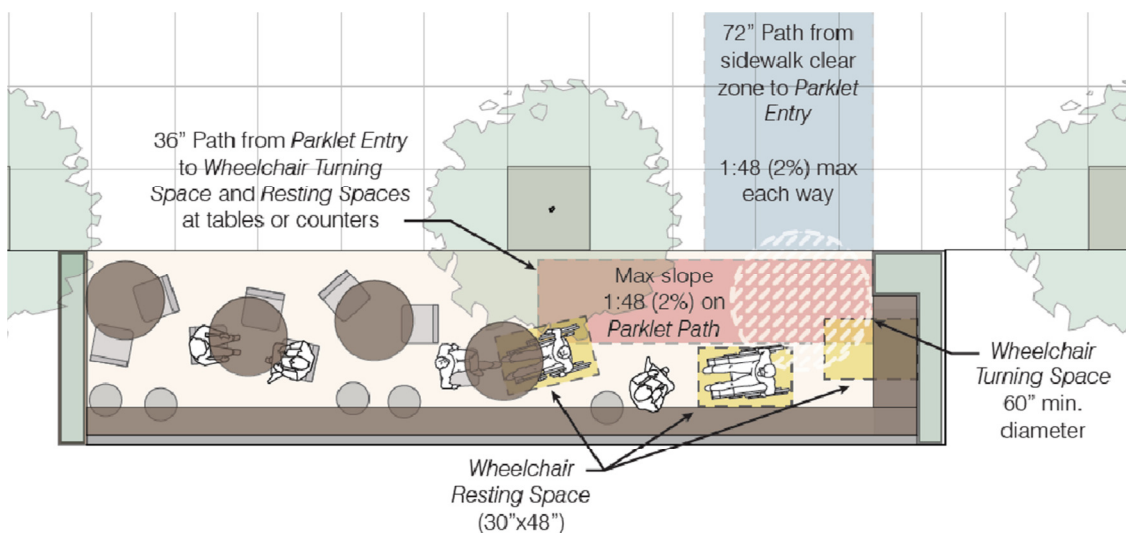


Integrated bicycle parking is strongly encouraged. Applicants may wish to integrate bicycle racks onto the exterior sides of the parklet platform if adequate distance is provided to adjacent parking spaces.

- **Incorporating bicycle parking** - Integrated bicycle parking is strongly encouraged. Bicycle parking can be incorporated into the parklet proposal in the following ways:
 - ❖ Custom bicycle racks integral to the parklet structure.
 - ❖ On the parklet platform. Applicants may wish to integrate bicycle racks onto the exterior sides of the parklet platform if adequate distance is provided to adjacent parking spaces.

VII. ACCESSIBILITY

- **Accessible Path of Travel** - Parklets are an extension of public sidewalk and pedestrian open space. All accessibility elements of the proposed parklet shall be designed, constructed and/or conform to the applicable provisions, rules, regulations and guidelines of the California Building Standard Codes and American with Disabilities Act.



VIII. SUSTAINABLE DESIGN

- **Locally sourced materials** - Sourcing locally produced materials for your parklet supports our local economy and reduces the embedded carbon footprint of the final structure by reducing transportation costs.
- **Low emission materials** - Choosing paints, stains, glues, and other materials that emit zero or low levels of volatile organic compounds (VOCs) helps improve air quality as well as the health of the people who are constructing your parklet.



- **Avoid plastic** - Plastic of any kind, including plexiglass, is strongly discouraged.
- **Materials that are easy to maintain** - Have a strategy for removing graffiti, and replacing or repairing damaged parklet features such as plants, railings, or other elements. Whereas some materials may cost more initially, they may ultimately save money in maintenance costs. For example, aluminum costs roughly three times as much as steel but when tagged, it can simply be cleaned with acetone. Project sponsors are ultimately responsible for making sure that their parklet is kept clean and in good repair.
- **No pressure treated wood or plywood** - Pressure treated lumber or plywood wood are not allowed in places where they will be visible. However, they can be used as part of the framing system.



IX. PARKLET APPLICATION PROCESS AND REQUIREMENTS

A. PARKLET PRE-APPLICATION REQUIREMENTS

A pre-application meeting is required with the Planning Division. Provide preliminary details and plans to help illustrate your proposal to determine feasibility of the proposed parklet. The pre-application should include the following information:

- **Location map** - Mapping/Aerial photo of property and adjacent improvements.
- **Photographs of Existing Site Conditions** - Document the existing conditions of existing sidewalks, curb conditions, nearby utilities, nearest cross-walk, streetscape, etc.
- **Design Concept** - Provide a quick sketch or photographs of similar parklet that portrays the design intent of the parklet.

B. PARKLET FORMAL SUBMITTAL REQUIREMENTS

Following the pre-application meeting with Planning Staff, then the Applicant may proceed to provide a formal submittal for review and approval. Complete applications will be reviewed within three working days. The following information will be needed at the time of formal submittal:

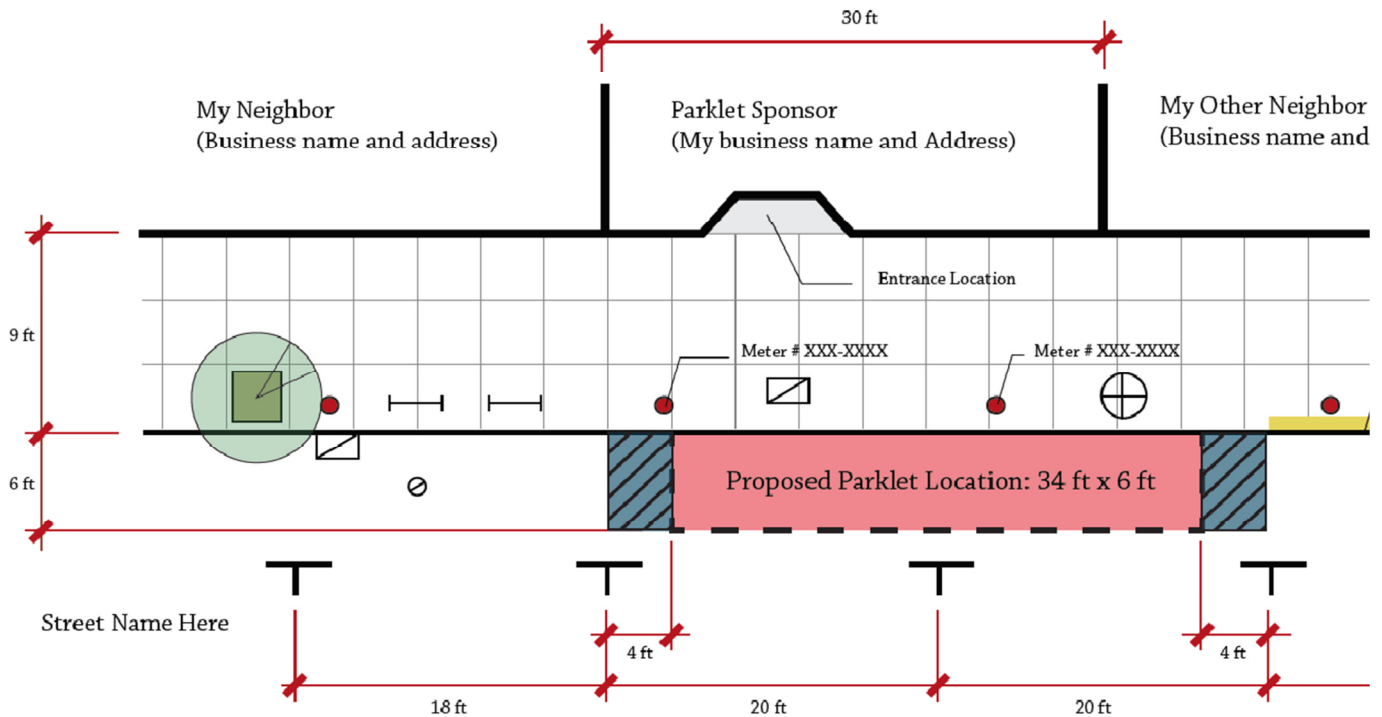
AGREEMENT

Land Use Application Form - Provide a completed land use application form available from the Planning Division. This form must be signed by the property owner and restaurant operator (if different from property owner).

Parklet Maintenance and License Agreement - A completed and executed license agreement shall be submitted with the Parklet submittal.

Permit Fee: \$849.02

SITE PLAN

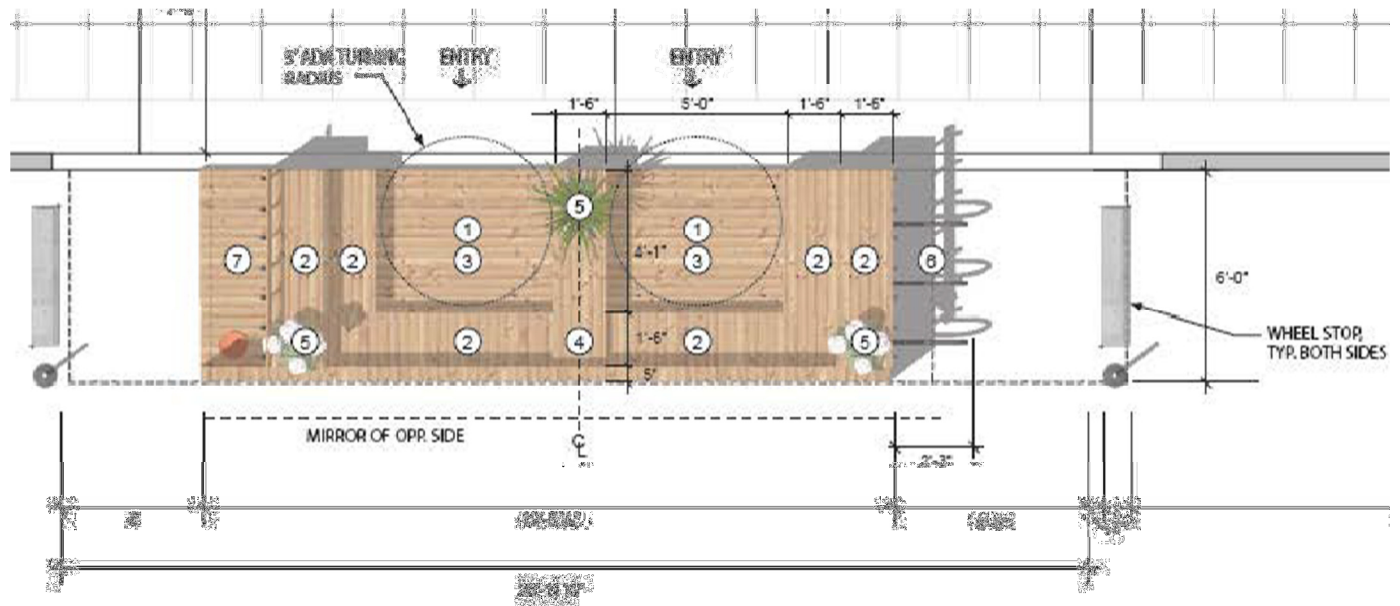


A dimensioned site plan showing the existing street, and sidewalk, and incidental equipment, utilities, etc, is required with all applications. The site plan should show the footprint of the proposed parklet, all street and sidewalk elements at least 20 feet on either side of the proposed parklet location. Site plans must be drawn to scale (by hand or computer), and shall include the following:

- Applicant’s building/tenant space and adjacent improvements
- Existing sidewalk width(s).
- Existing curb cuts and driveways.
- Adjacent bicycle lane or auto traffic lane.
- Existing parking spaces with dimensions.
- North arrow for orientation.
- Other existing sidewalk features near the proposed
- parklet area (fire hydrants, streetlights, utility access panels, bicycle racks, etc).
- Proposed parklet footprint and dimensions.
- Parklet setback dimensions (36” inches from adjacent parking spaces and 24” from adjacent bicycle lane or auto traffic lane).

FLOOR PLAN

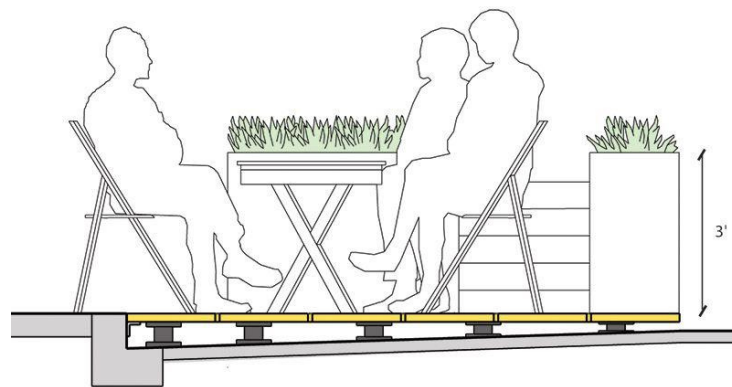
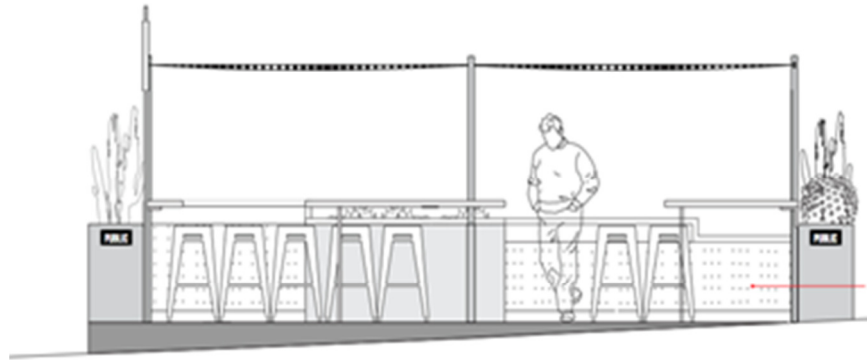
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A floor plan showing the existing street, sidewalk, adjacent parking spaces and parklet floor layout and design. Floor plans must be drawn to scale (by hand or computer) and shall contain the following information:

- Applicant’s building with tenant space and use identified, and 20’ beyond on both sides
- Existing sidewalk width(s).
- Existing curb cuts and driveways
- Adjacent bicycle lane or auto traffic lane
- Wall/Fencing layout
- Full Dimensions

ELEVATIONS



Elevations showing the proposed wall/rail design existing street, and sidewalk, and incidental equipment, utilities, etc, are required with all applications. Elevations must be drawn to scale (by hand or computer), and shall contain the following information:

- Street elevation
- Sidewalk elevation
- Proposed materials
- Railing design and materials
- Planters
- Lighting



C. PERMIT APPROVAL, PARKLET CONSTRUCTION, AND INSPECTION

Once Parklet has been approved and the licensing agreement recorded, the Applicant may construct the parklet. The Development Services Department will inspect the parklet prior to occupancy to confirm the design is consistent with the approved plans.