



TEAL CLUB SPECIFIC PLAN

Prepared for:
City of Oxnard
Development Services

Prepared by:
Development Planning Services, Inc.
Camarillo, California 93012

May 2024





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1

INTRODUCTION

1.1 PURPOSE OF THE SPECIFIC PLAN

The Teal Club Specific Plan provides the City of Oxnard with a comprehensive planning document to direct the orderly development of the 149.7-acre property known as the Teal Club Planning Area (see Figures 1-1 and 1-2). The Specific Plan provides a conceptual land use plan, development standards, guidelines, and programs to ensure that this area of the city is developed in a manner consistent with the goals, objectives, principles, and policies of the City of Oxnard 2030 General Plan (referred to in this document as the General Plan).

The Specific Plan allows development of a variety of residential, mixed-use, neighborhood commercial, industrial and recreational uses within the Specific Plan area. The guidelines and standards contained in this Specific Plan will ensure that these uses, and the associated infrastructure elements and public spaces are planned and designed in an integrated manner.

Creating a walkable Urban Village is a primary goal of this Specific Plan. In order to ensure long-term sustainability the Specific Plan shall incorporate policies of both the State of California SB 375 (Greenhouse Gas Bill) and “Build It Green” design standards. The plan includes a strategically-placed

Transit Hub with bus turn-out and shelter and bike racks at Ventura Road and Beverly Drive.

1.2 AUTHORITY & SCOPE

California Government Code (Sections 65450 through 65457) authorizes the preparation and adoption of specific plans. The Teal Club Specific Plan has been prepared in accordance with the requirements of this code as summarized below.

1.2.1 CALIFORNIA GOVERNMENT CODE COMPLIANCE

California Government Code Section 65450 gives a legislative body the authority to prepare a specific plan for the systematic implementation of the General Plan for all or part of the area covered by the General Plan. As outlined in the Government Code Sections 65451 and 65452, specific plans are to contain a text and diagram or diagrams specifying the following:

The italicized text indicates where in the Teal Club Specific Plan these requirements have been addressed.

- A statement of the relationship of the Specific Plan to the General Plan. *Chapter 9 describes how this Specific Plan relates to, and is in conformance with, the goals and objectives of the City of Oxnard General Plan;*
- The distribution, location, and extent of land uses. *The goals of the proposed Specific Plan, as defined in Section 1.4, provides the*



rationale for the proposed land uses and their distribution throughout the project site. Section 3.2 contains a detailed land-use table (Table 3-1) and precise land use plan (Figure 3-1). The project includes approximately 990 residential units, approximately 8.8 acres commercial mixed-use, approximately 9.1 acres of Business Research uses, and approximately 17.7 acres of parks and greenbelts.

- *The distribution, location, and extent of major infrastructure improvements needed to support the land uses described in the plan. Section 4.2 describes the circulation plan for the site, including street sections. Potable water, wastewater, and stormwater infrastructure as well as utilities are outlined in Section 4.3 of the proposed Specific Plan.*
- *Development standards and criteria. Section 3.4 and Table 3-4 identifies development standards. Sections 3.5 through 3.8 identifies project design objectives and guidelines;*
- *A program of phasing and implementation measures. Chapter 8 identifies implementation and administration for this Specific Plan.*

1.2.2 CITY OF OXNARD ZONING CODE COMPLIANCE

A key function of the Specific Plan is to reduce the need for subsequent master planning and environmental review procedures as the project area is developed. The Specific Plan fixes the general layout and configuration of streets and defines the land uses allowed in the Specific Plan area. At the time of subdivision maps will precisely fix the boundary of the land uses, the location of streets, and the configuration of residential

lots. After adoption of the Specific Plan, either by ordinance or resolution, a Specific Plan supplements existing zoning regulations and has an effect similar to the local General Plan. For example, the California Subdivision Map Act requires the legislative body to deny approval of a tentative or final subdivision map if it is not in substantial conformance with the applicable specific plan. In addition, a development agreement between a municipal body and a developer/applicant cannot be approved unless the legislative body finds the agreement is consistent with the General Plan and any applicable specific plans.

1.3 PROJECT SITE LOCATION

1.3.1 PHYSICAL SETTING

The Teal Club Specific Plan area project site is located in the western portion of Oxnard; Ventura County, California. A majority of the site is currently outside of the City limits and will require annexation to the City of Oxnard. The plan area is surrounded by the City of Oxnard on three sides and a mix of existing development uses on the north, east, and south, with County agricultural uses to the west. The site is bounded by existing roads on all sides: Ventura Road to the east, Teal Club Road to the south, Patterson Road to the west, and Doris Avenue to the north. The Oxnard Airport is located immediately to the south of the project. The project site and surrounding land uses are shown on Figure 1-3.

The Teal Club Specific Plan project area is within the City of Oxnard Sphere of Influence and inside the Oxnard “City Urban Restriction Boundaries” (CURB) boundary of the City. As a result of this location the

FIGURE 1-1 REGIONAL LOCATION & CURB BOUNDARIES

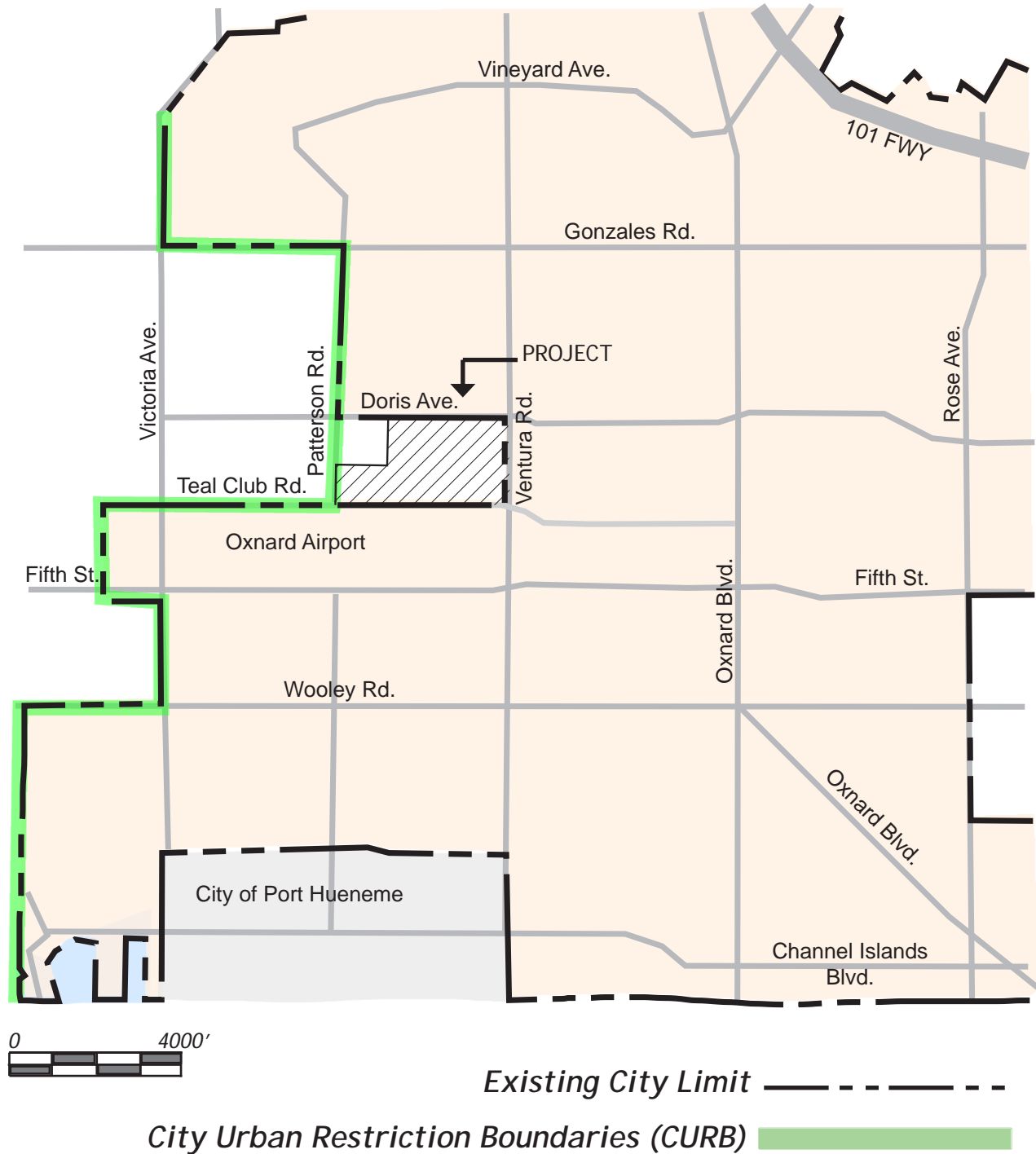
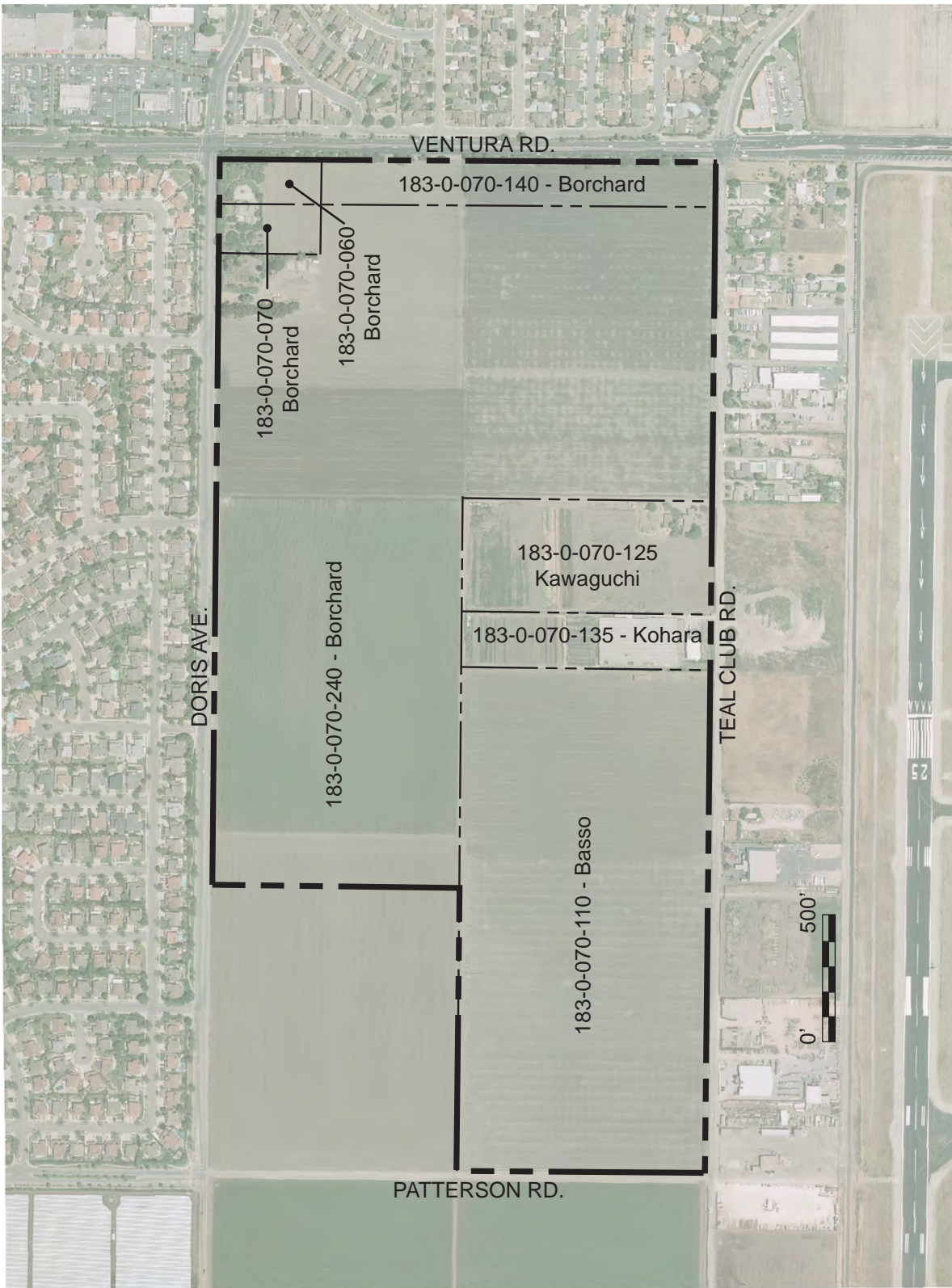


FIGURE 1-2 EXISTING SITE - ASSESSOR PARCELS



Specific Plan area is not within an area subject to either City of Oxnard or Ventura County “Save Open Space and Agricultural Resources” (SOAR) voter approval. The CURB boundary is shown on Figure 1-1.

1.3.2 EXISTING LAND USES & ON-SITE CHARACTERISTICS

The proposed project site encompasses seven parcels totaling 149.72 acres of land, including the following APN #'s (illustrated in Figure 1-2):

- 183-0-070-060;
- 183-0-070-070;
- 183-0-070-240;
- 183-0-070-110;
- 183-0-070-125;
- 183-0-070-135;
- 183-0-070-140.

The site's topography is primarily flat and characterized by row crop areas with primary drainage trending from north to south.

Utilities

There are no existing public utilities within the Teal Club Specific Plan area. Along all

four streets that surround the plan area there are utilities and infra-structure that serve existing development within the City of Oxnard. The four streets are Teal Club Road on the south, Patterson Road on the west, Doris Avenue on the north, and Ventura Road on the east. Southern California Edison local service and/or distribution lines exist on Teal Club, Patterson, and Ventura Roads. Southern California Gas distribution lines are located in Teal Club Road, Doris Avenue, and Ventura Road. City of Oxnard water lines exist in Teal Club Road, Doris Avenue, and Ventura Road; a reclaimed water line is in Ventura Road. City of Oxnard sewer mains are located in Patterson and Ventura Roads. The only storm drainage facilities in the immediate area are varying depth agricultural open drains; the most substantial one is located on the north side of Teal Club Road.

Historical Use & Preservation

The majority of the Teal Club property has been used for commercial farming operations for the past 50 years or longer and is actively farmed today. Farming activities have historically included various row crops, citrus and avocado trees.

A farm house and related out-buildings are located near the northeast corner of the site and will be evaluated for historical significance through the Environmental Impact Report.

Biology

Due to many years of crop production, most of the ground surface has been

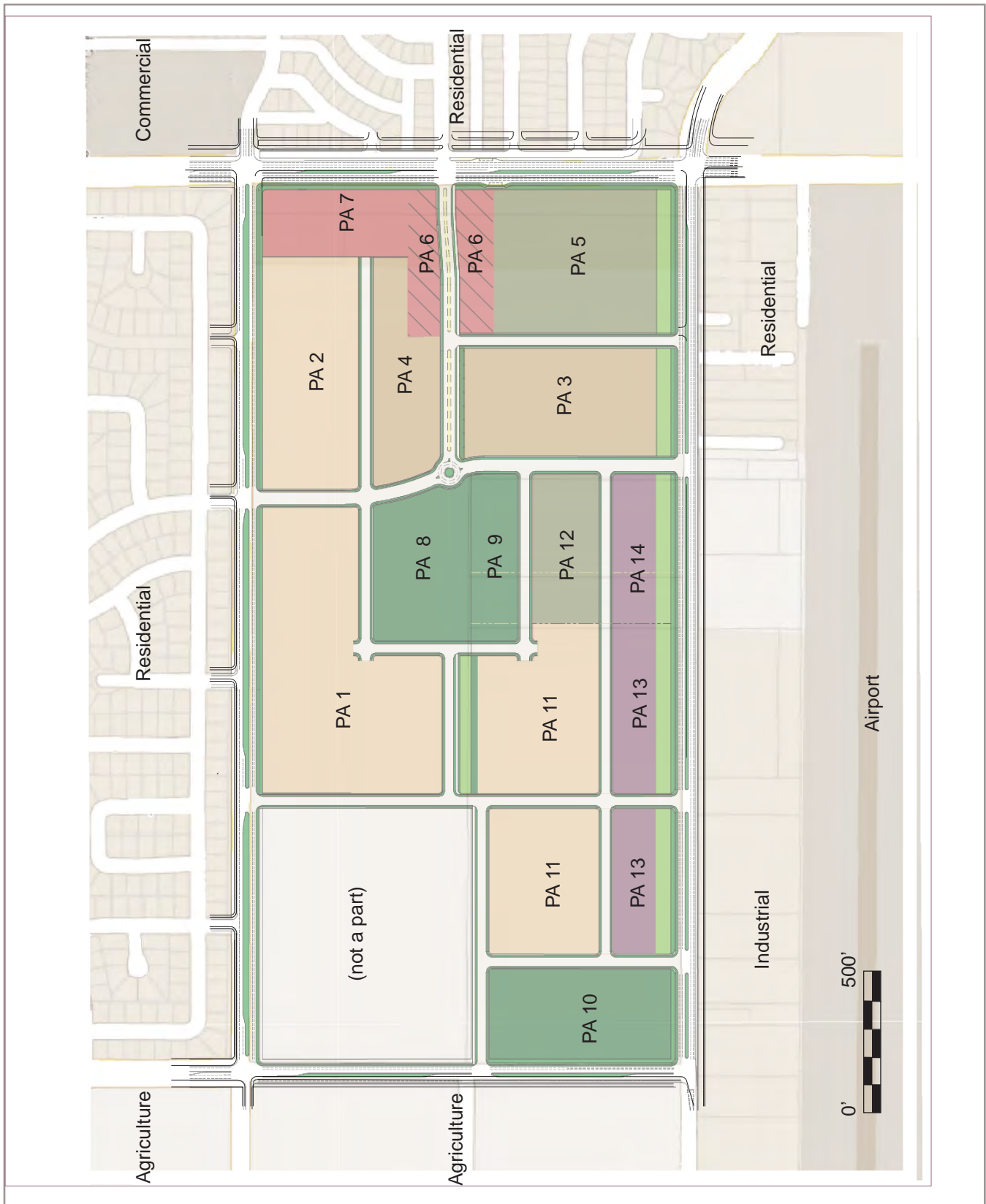


TEAL CLUB RD. LOOKING WEST





FIGURE 1-3 SURROUNDING LAND USES



PA 1
Detached Cluster

PA 2
Detached Cluster

PA 3
Townhome
Coronado Place

PA 4
Townhome

PA 5
Apartment /Condo

PA 6

PA 7

PA 8

PA 9

PA 10

PA 11
Attached Condo

PA 12
Apartment /Condo

PA 13

PA 14

DORIS AVE.

VENTURA RD.

PATTERSON RD.

TEAL CLUB RD.

Note: Teal Club Plan is designated as a City of Oxnard Urban Village

0' 500'

Transit Hub

Legend:

- Residential Low Medium
- Residential Medium
- Residential Medium-High
- Park
- General Commercial
- Mixed Use
- Business Research Park
- Stormwater Treatment & Detention Basin

disturbed and very little native vegetation remains within the farmed areas. The site does not contain any suitable habitat areas or features other than the few ornamental trees located near existing structures and along Ventura Road.

1.4 PROJECT GOALS & OBJECTIVES

1.4.1 GOAL #1: CREATE A COMPREHENSIVE URBAN VILLAGE DEVELOPMENT PLAN FOR THE TEAL CLUB PROJECT AREA.

Objectives:

- Create an integrated land, transportation, and infrastructure plan that allows for a mix of residential, mixed-use and commercial development to minimize the need for short distance vehicle trips both within the project and within the City.
- Provide a balance and economic match, to the extent feasible, between on-site housing and nearby employment opportunities.
- Create an integrated vehicular, pedestrian and bicycle circulation system that connects residential, industrial and commercial uses within the project.
- Establish land uses that permit a range of housing opportunities with varying densities, types, styles, prices and tenancy characteristics including compliance with the 2030 General Plan Housing Element requirements.
- Adopt design guidelines and regulations to provide for consistent and orderly implementation of the plan.

1.4.2 GOAL #2: INTEGRATE THE NEW COMMUNITY WITHIN THE EXISTING NEIGHBORHOOD.

Objectives:

- Provide development and transitional land use patterns that do not conflict with surrounding land uses or neighborhoods.
- Avoid leapfrog development and accommodate projected growth in a location adjacent to existing infrastructure, urban services, existing circulation elements and community facilities.
- Establish development regulations to ensure residential neighborhoods are compatible with the surrounding area and all proposed land uses are properly integrated or buffered, as appropriate.
- Provide an adequate buffer between the project area and agricultural uses to the west on both an interim and permanent basis, as needed.
- Provide bike and trail linkages between the project area and existing facilities.
- Provide land for a potential fire station or sub-station, if determined necessary.



1.4.3 GOAL #3: CREATE A FISCALLY RESPONSIBLE COMMUNITY.

Objectives:

- Provide a mix of land uses that are financially independent, sustainable, and not a fiscal burden on the General Fund of the City of Oxnard.
- Ensure appropriate phasing and financing for community facilities including street and road improvements, water, urban runoff and flood control facilities and parks.
- Create neighborhoods with lasting value by setting high quality standards for residential and commercial land development and related public improvements.
- Development shall comply with the State of California's "Build it Green" standards.

1.5 SCOPE & FORMAT

The Teal Club Specific Plan is divided into the following chapters:

- Chapter 1 - Introduction: This section provides an overview of the project and describes the purpose and intent of the Specific Plan document.
- Chapter 2 - Sustainable Community: this section addresses how the Teal Club Specific Plan sets standards for addressing climate change and SB 375 compliance.
- Chapter 3 - Community Development: This section establishes the design objectives, guidelines and development standards associated with the project.
- Chapter 4 - Infrastructure and Community Services: This section identifies the on- and off-site necessary infrastructure that link the project to the surrounding city.
- Chapter 5 - Environmental Resources: Working in conjunction with Chapter 2 - Sustainable Community, this section provides information on the stewardship of additional environmental resources.
- Chapter 6 - Safety and Hazards: This section discusses the compatibility with adjacent uses and safety issues.
- Chapter 7 - Housing Section: This section establishes the Urban Village concept and affordable housing requirements of the project.
- Chapter 8 - Implementation: This section provides detailed information on the phasing plan, administration and Specific Plan processes.
- Chapter 9 - General Plan Consistency: This section identifies how the Specific Plan relates to, and is in conformance with, the applicable goals and objectives identified in the City of Oxnard General Plan.

2

SUSTAINABLE COMMUNITY

2.1 OVERVIEW

This chapter specifically focuses on issues relating to climate change and the implementation of SB 375. Consistent with the organization of the General Plan, additional sustainability issues are addressed in Chapter 5 - Environment Resources.

The Teal Club Specific Plan approaches sustainability as a holistic process. Sustainability begins with site selection, takes form through planning and construction and is completed with testing and monitoring. The Teal Club Specific Plan's sustainability goal is to create a "low impact" community that effectively balances resources and provides for long-term sustainability. Technologies are rapidly evolving and this plan does not intend to preclude new, efficient options to achieve sustainability.

2.2 LAND USE & TRANSPORTATION

The first step toward sustainability is site selection. The Teal Club site is ideally located adjacent to established infrastructure with convenient access to public transportation, schools, bike paths, major arterials, employment centers, and shopping. This location will help minimize vehicle miles traveled.

2.2.1 JOBS & HOUSING PROXIMITY

In addition to creating a balance of jobs and housing within and near the planning area, the Teal Club Specific Plan is located near major employment centers including Naval Base Ventura County, the Port of Hueneme and major commercial areas.

2.2.2 TRANSIT-ORIENTED DEVELOPMENT

The Teal Club Specific Plan is located directly adjacent to Ventura Road, a Gold Coast transit corridor, and will include an on-site public transit stop. Gold Coast provides direct service from the Teal Club Specific Plan area to major employment centers at the Esplanade, Naval Base Ventura County and Oxnard College, with connecting Gold Coast service throughout the city and connections to the Ventura County Transportation Commission (VCTC) Highway 101 Corridor route to Ventura, Camarillo and Thousand Oaks. The Oxnard Transportation Center is only 1.25 to 1.75 miles from the entire site via connecting bus service or pedestrian sidewalks and provides Metrolink and Amtrak service. East of the Specific Plan area the City of Oxnard Bicycle Facilities Master Plan map identifies existing Class II bike lanes on Doris Avenue and recommended Class II future bike lanes on Fifth Street, also providing access to the Oxnard Transportation Center.

2.2.3 INTEGRATED LAND USES

A variety of land use types are included within the Specific Plan including various residential densities, retail, commercial and open space uses. The uses are all interconnected with trails, sidewalks, and bikeways to minimize the number of vehicle miles traveled. Figure 4-11 illustrates the onsite Bike Path Circulation Plan.



2.2.4 DENSITY

The site's location on a transit corridor provides an opportunity for compact development with increased residential densities in appropriate locations near the transit stop.

2.3 ENERGY GENERATION & CONSERVATION

2.3.1 SOLAR ENERGY & TITLE 24

All newly constructed buildings within the Teal Club Specific Plan area shall be equipped with solar panels and meet all applicable State of California energy-conservation requirements of Title 24 at the time of issuance of building permits. All development within the Teal Club Specific Plan shall comply with the California Energy Commission rules and requirements applicable at the time of issuance of building permits.

2.3.2 BEYOND GREEN CODE

The intent of the Teal Club Specific Plan is to create a community that meets current sustainable building standards. To achieve this, it is required that all new construction receive "Greenpoint" ratings through the California "Build It Green" program or a similar program that quantifies and documents green building standards.

2.3.3 PEV CHARGING STATIONS

Detached residential units shall be wired to allow for charging plug-in electric vehicles. Multi-family and commercial buildings shall provide for shared PEV charging stations.

2.4 GHG MITIGATIONS

The primary greenhouse gas (GHG) mitigations are addressed in the compact, walkable design, proximity to public transit and requirements for "green" building practices of the Teal Club Specific Plan. Additional strategies to reduce GHG in the environment include:

2.4.1 URBAN FORESTRY

In addition to the aesthetic benefits to the community, planting trees has a number of beneficial results. Trees and vegetation temporarily sequester carbon dioxide and reduce other air pollutants. When strategically located, trees can also provide shading that reduces electrical use and minimizes potential heat island effects. The landscaping guidelines of the Teal Club Specific Plan, outlined in Section 3.7.7, encourage maximizing the number of trees selected for minimal maintenance and planted strategically for shade.

The neighborhood park, storm-water detention basins, agricultural buffers, vegetated swales and other landscaped areas will also be utilized for carbon sequestration.

2.4.2 OTHER GHG EMISSIONS

A number of additional strategies are included in the design and operation of the Teal Club Specific Plan to minimize GHG impacts and are quantified in the project EIR and Mitigation Monitoring Program.

Construction

During the construction phases of the project, exhaust emissions reductions will be required. These include the use of alternative fuel, electric equipment, diesel particulate filters, diesel oxidation catalysts, and new tier engines. Fugitive dust emissions reductions including construction site watering is required. The Teal Club Specific Plan EIR Mitigation Monitoring Program shall provide documentation of best practices to reduce emissions and impacts.

Architectural Coatings

Low volatile organic compound (VOC) coatings are required in all new construction.

Water

As discussed in the Infrastructure section of the Specific Plan, the potable water is sourced and treated locally, thereby reducing indirect GHGs. “Advanced Water Purification System” water will be delivered to the site through the City of Oxnard existing Ventura Road water lines and utilized throughout the project site.

Waste

Mandatory city recycling and green waste composting programs will be implemented throughout the project in compliance with City of Oxnard requirements throughout the life of the project.



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3

COMMUNITY DEVELOPMENT

3.1 PLANNING CONTEXT

The Teal Club Specific Plan site is one of the last major properties located within the City of Oxnard Sphere of Influence, established by the Ventura County Local Agency Formation Commission (LAFCO), and the City Urban Restriction Boundary (CURB) and has been designated for urban growth for many years. Figure 1-1 illustrates the CURB boundary and existing City of Oxnard boundary.

The Specific Plan land use conforms to the City's 2030 General Plan land use and density designation for the site. The City of Oxnard General Plan 2013-2021 Housing Element identifies the Teal Club Specific Plan site for development, and the 2021-2029 Draft Housing Element anticipates development of the Teal Club Specific Plan site. The State of California Department of Housing & Community Development (HCD) approved the 2013-2021 City of Oxnard Housing

Element with the inclusion of the Teal Club Specific Plan development. It is the only undeveloped site of its size within the Sphere of Influence and within the CURB boundary surrounded on three sides by industrial, residential and commercial land uses and adjacent to all municipal services. The site therefore provides an excellent location for a planned, orderly and efficient extension of existing urban development.

The Teal Club Specific Plan site is unique in that it is on an existing public transit corridor, adequately served by the City's existing sewer and water system and within walking distance of a variety of retail stores and support services. The city and county have an extensive agricultural land protection program that precludes local agencies from re-zoning the county's agricultural land without a public vote to allow land use changes to non-agricultural uses. When voters approved the city's Save Open Space and Agricultural Resources (SOAR) initiative in 1998, the Teal Club site was specifically located inside the urban growth boundary (CURB). The Ventura County SOAR initiative has been extended by voter approval to 2050. This county-wide land use policy continues the protection of agricultural resources and requires local planning agencies to consider how to best utilize the remaining infill properties to meet the housing needs of the coming decades. The Teal Club Specific Plan is intended to establish a land use plan that provides for future housing needs while being compatible with the existing surrounding land use and development patterns.

The project land Uses are listed in Table 3-1; the Land Use Plan is illustrated in Figure 3-1; A statistical summary of the land uses is provided in Table 3-2



3.2 LAND USE PLAN

The Teal Club Specific Plan envisions a pedestrian-oriented village that links to the existing adjacent community and provides for distinct residential neighborhoods serving as an Urban Village and containing a transit hub and a mix of commercial uses, employment centers, and public and recreation facilities. A ten-acre neighborhood park and a 7-plus acre park will serve new and existing neighborhoods. Public road improvements, shared HOA-maintained and operated private recreational facilities and an east-west internal multi-use public path are proposed.

The Oxnard School District has purchased an adjacent 25-acre parcel for the purpose of constructing K-8 school facilities. The school parcel is not a part of the Teal Club Specific Plan and has been evaluated under a separate EIR certified by the Oxnard School District.

Each residential neighborhood is planned to be within a half-mile radius of employment centers and/or neighborhood retail uses. All neighborhoods will be connected by pedestrian paths, bike lanes and/or easily-accessed neighborhood sidewalks. Class I and Class II bike routes are included within the Specific Plan area, and all perimeter streets will include Class II bike lanes, consistent with the City of Oxnard Bicycle & Pedestrian Master Plan.

The Specific Plan provides a variety of housing types and residential densities including both market-rate and income-restricted affordable housing within the plan area. For-sale housing will range from multi-family units to small lot, single-family homes on lots up to 3,500 square feet in size. The variety of housing types and densities will provide for a wide price range for multiple household types and incomes to meet a

portion of the future housing needs of the City of Oxnard.

The Urban Village concept envisions a highly visible, pedestrian-accessible central multi-use path to encourage walking and biking within the community. The Ventura Road frontage north of Beverly Drive will contain commercial and retail businesses. A mixed-use gateway is planned for the Beverly Drive entry area immediately west of Ventura Road. The retail center and mixed use area, containing a public plaza, will be within walking distance to all new residential neighborhoods and is adjacent to a main line transit stop for access to nearby employment centers such as the Esplanade, Naval Base Ventura County and Channel Islands Harbor. A business/research park on the south side of the project would provide another source of potentially “walkable” jobs within the planning area.

The centrally-located ten-acre neighborhood park is intended to be a gathering place and a healthy activity zone for the entire community. Concurrent with residential construction of Phase 1 of the Specific Plan the developers shall provide 6.5 acres of the ten-acre park and construct the Phase 1 park improvements as required by the Phase 1 Development Agreement.. Programming for this park will be determined by the Oxnard Recreation and Community Services Department prior to approval of Phase 1 development entitlements. Development of Phase 2 shall provide an additional 3.5 acres, with park completion as required by future Phase 2 project Conditions of Approval.

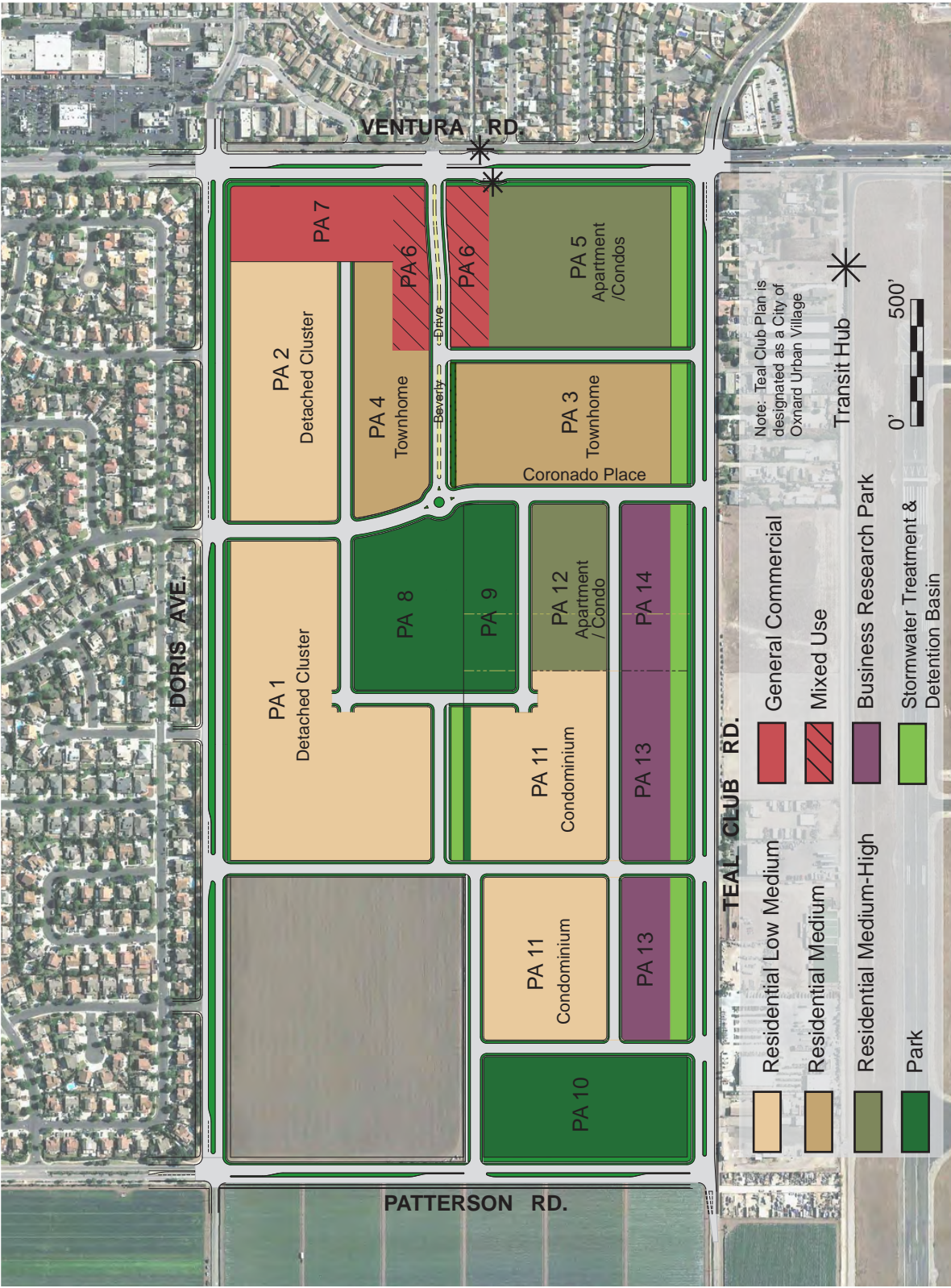
Programming in the ten-acre park will consist of both active recreation elements (game courts, game fields and tot lot/playground areas) and passive recreation elements (benches, sidewalks

**Table 3-1
Land Use Table**

Land Use	Maximum Density	TOTAL PLANNED			
		Acres (1)	Units (2)	Density	Bldg. Sq. Ft.
RESIDENTIAL USES & LAND USE CATEGORY:					
PA-1 Detached Cluster - Residential Low Medium	10 DU/Ac.	17.52	140	8 DU/Ac.	-
PA-2 Detached Cluster - Residential Low Medium	10 DU/Ac.	10.01	80	8 DU/Ac.	-
SUBTOTAL Detached Cluster Units	-	27.53	220	-	-
PA-3 Attached or Detached Townhome - Residential Medium	18 DU/Ac.	9.6	145	15 DU/Ac.	-
PA-4 Attached or Detached Townhome - Residential Medium	18 DU/Ac.	5.54	88	16 DU/Ac.	-
PA-5 Attached Apartment/Condo - Residential Medium High	30 DU/Ac.	10.57	240	23 DU/Ac.	-
PA-11 Attached or Detached Condo - Residential Low Medium	12 DU/Ac.	15.64	167	11 DU/Ac.	-
PA-12 Attached Apartment/Condo - Residential Medium High	30 DU/Ac.	4.43	100	23 DU/Ac.	-
Commercial/Mixed Use (Residential component)	-	0.00	30	-	-
SUBTOTAL Multi-Family	-	45.78	770	-	-
TOTAL RESIDENTIAL	-	73.31	990	-	-
PARKS & OPEN SPACE					
PA-8 Community Park - Phase 1 Portion	-	6.50	0	-	-
PA-9 Community Park - Phase 2 Portion	-	3.50	0	-	-
PA-10 Community Park - Phase 2	-	7.38	0	-	-
Beverly Dr. Greenbelt	-	0.38	0	-	-
SUBTOTAL Parks & Open Space	-	17.76	0	-	-
COMMERCIAL/MIXED USE					
PA-6 Commercial/Mixed Use - General Commercial	-	4.35	0	-	10,000
PA-7 Village Commercial - General Commercial	-	4.43	0	-	50,000
SUBTOTAL Commercial/Mixed Use	-	8.78	0	-	60,000
LIGHT INDUSTRIAL					
PA-13 Business Research Park	-	6.19	0	-	88,000
PA-14 Business Research Park	-	2.92	0	-	44,000
SUBTOTAL Light Industrial	-	9.11	0	-	132,000
ARTERIAL ROADWAYS					
Ventura Road	-	2.82	0	-	-
Doris Avenue	-	2.80	0	-	-
Patterson Road	-	0.30	0	-	-
Teal Club Road	-	7.20	0	-	-
SUBTOTAL Arterial Roadways	-	13.12	0	-	-
INTERIOR ROADWAYS & MISC.					
Interior Roadways show in Fig 2-1	-	22.18	0	-	-
Stormwater Treatment / Detention Basins	-	5.46	0	-	-
SUBTOTAL Interior Roadways & Misc.	-	27.64	0	-	-
TOTAL PROJECT	-	149.72	990	-	192,000

1. Land Use Acres shown in this table are Net Area, measured from the right-of way line of streets shown in the Teal Club Specific Plan "Precise Land Use Plan", Figure 2-1.
2. The number of units within any Residential land use area may be refined during the course of subsequent City review and approval, provided that the total number of units within the Specific Plan does not increase beyond that shown in this table, or unless subsequent approval includes a density bonus and additional CEQA review as applicable under State or local law.

FIGURE 3-1 LAND USE PLAN



**Table 3-2
Land Use Statistical Summary**

Land Use Description	Planning Area	Acres	DU	SF	% of Total Plan
Recreation/Open Space Uses:					
Neighborhood Parks	PA 8-10	17.38			
Greenbelt		.38			
<i>Recreation/Open Space Subtotal:</i>		<i>17.76</i>			<i>11.9%</i>
Developed Uses:					
Mixed-Use, Village Commercial	PA 6 & 7	8.78	30	60,000	
Business Research Park	PA 13 & 14	9.11		132,000	
Medium-High Density	PA 5 & 12	15.0	340		
Medium Density	PA 3 & 4	15.14	233		
Low-Medium Density Residential	PA 1,2 & 11	43.17	387		
<i>Developed Uses Subtotal:</i>		<i>91.2</i>	<i>990</i>		<i>60.9%</i>
Streets & Misc.:					
Arterial Roadways		13.12			
Interior Roadways		22.18			
Detention Basins/Detention Basins		5.46			
<i>Streets & Misc. Subtotal:</i>		<i>40.76</i>			<i>27.2%</i>
Project Total:		149.72	990	192,000	100.00%



and small-scale picnic areas). Parking is intended to be primarily along the perimeter of the park, in either parallel or angled parking stalls along the street, in order to maximize the usable park area. Maintenance of the neighborhood park shall be funded by a financing district formed by the developers of the Specific Plan and financed by all development within the Specific Plan as provided for in the project Development Agreement(s).

An additional 7.3 acre park is included in Phase 2 at the western edge of the Specific Plan, at the northeast quadrant of the intersection of Teal Club Road and Patterson Road. Programming for this park will be determined by the Oxnard Recreation and Community Services Department prior to approval of Phase 2 development entitlements. Concurrent with construction of Phase 2 of the Specific Plan the developers shall provide 7.3 acres of land with park completion as required by future Phase 2 development project Conditions of Approval.

Maintenance of the Phase 2 neighborhood parks shall be funded by a financing district formed by the developers of the Specific Plan and financed by all developments.

Additional private recreation and common area open space amenities, including seating areas, barbecues and picnic tables, shall be located within the Medium Density neighborhoods of the plan area. The Medium-High Density planning area shall include a pool, seating areas, barbecues and picnic tables for use by the residents of that planning area. Collectively these public and HOA-member private amenities will ensure that outdoor recreational opportunities are located within a quarter mile walking distance of all residential units via the public multi-use trail and sidewalk network.

TABLE 3-3 ZONING		
LAND USE DESCRIPTION	ZONING	DENSITY (DU/AC)
Low-Medium Residential - Detached Cluster	R-2	7-12
Low-Medium Residential - Attached or Detached Cluster	R-2	7-12
Medium Residential - Attached & Condominium	R-3	12-18
Medium-High Density Residential - Apartment/Condo	R-4	18-30
Mixed-Use Commercial (1st Floor)	C-2	N/A
Mixed-Use Residential (2nd/3rd Floor)	C-2	12-18
Village Commercial & CPD Uses	CPD	N/A
Business Research Park	BRP	N/A
Park	C-R	N/A
Greenbelts & Open Space	C-R	N/A

3.3 PROPOSED ZONE CLASSIFICATIONS & INCLUSIONARY HOUSING

3.3.1 ZONE CLASSIFICATIONS

Table 3-3 defines zoning classifications proposed within the Specific Plan area. In categories where this plan does not establish or define standards, the standards of the City of Oxnard Zoning Ordinance shall apply. All uses within the zoning classifications of this Specific Plan shall be consistent with those allowed in Chapter 16 of the Oxnard Municipal Code. Special Use Permits shall be allowed as defined in Section 8 of this Specific Plan

3.3.2 INCLUSIONARY HOUSING

The Teal Club Specific Plan includes a range of housing types. Chapter 7 provides additional housing information. In accordance with the Housing Element of the City's General Plan and the Urban Village criteria, the Teal Club Specific Plan residential component will require that 15% of the total residential development is Affordable Housing for very low and lower-income workforce households (defined as Very Low and Low Income Households). Affordable Housing Agreements shall be executed and covenants recorded as necessary to insure that all Affordable Housing units within the Specific Plan remain affordable for a period of 55 years.

3.4 DEVELOPMENT STANDARDS

Table 3-4 defines the Development Standards for the Teal Club Specific Plan. An overall objective of this Specific Plan is to create a walkable neighborhood with highly visible

pedestrian connections in an open, visually pleasing setting. The Development Standards are established to provide objective standards governing development of each of the Planning Areas. The Development Standards ensure that each neighborhood is developed consistent with the Zone Classifications outlined in Section 3.3 and within the density range of development in each Planning Area. For each Zone Classification the Development Standards define the allowable building height, setback requirements and other standards to be met in site planning for each Planning Area.

Any deviations from these standards in the residential Zone Classifications shall require approval of a Residential Planned Development (RPD) Permit as defined in City of Oxnard Municipal Code Section 16-450. In the BRP and Commercial/Mixed Use areas a Development Design Review Permit is required for any deviations from these standards. The Setback Requirements of this Specific Plan shall be applied as the minimum requirements for projects within the Specific Plan, unless modified by an RPD Permit.

The City of Oxnard Municipal Code shall regulate development in the Teal Club Specific Plan, except as modified by the Specific Plan text, conditions, regulations and standards contained herein. In such cases where Teal Club Specific Plan text, conditions, regulations and standards conflict with those in sections contained in the City of Oxnard Municipal Code, the Teal Club Specific Plan text, conditions, regulations and standards shall apply.

3.5 DESIGN OVERVIEW

This section discusses and expands upon the development plan identified in Section 2. The design philosophy and objectives outline the



Urban Village design of the Teal Club Specific Plan area. The design guidelines establish concepts and the range of features to be considered in the design of future residential, commercial, mixed-use and industrial land uses. These guidelines can be implemented in a variety of ways and are written to provide architectural flexibility and encourage diversity in architecture.

3.6 DESIGN PHILOSOPHY & OBJECTIVES

The Teal Club Specific Plan seeks to create a new community that carefully integrates multiple land uses in a walkable setting. The entire plan area is linked together by the east /west multi-use trail, Class II bike lanes, and neighborhood sidewalks. The design incorporates Urban Village concepts with an emphasis on highly-visible pedestrian routes and the interconnection of residential neighborhoods, community-serving commercial uses, recreational opportunities, the adjacent Oxnard School District educational facilities and on-site public transportation access. The bike path circulation plan is illustrated in Figure 4-11.

The Teal Club Specific Plan encourages commercial spaces and residential neighborhoods with a strong sense of identity through planning and landscape architecture emphasizing the Mediterranean climate as well as architecture that complements the design character of Coastal California. To accomplish this, the design guidelines for the project draw heavily from neighborhoods having stood the test of time and known for their beauty and ambiance, particularly the Henry T. Oxnard Historic District in the nearby Wilson Neighborhood and the older coastal residential neighborhoods of Santa Barbara and Carpinteria, California.

3.6.1 PEDESTRIAN-ORIENTED DESIGN OBJECTIVES

The pedestrian-oriented design objectives play a significant part in defining this Specific Plan by facilitating direct connections between the individual residential neighborhoods, commercial and mixed-use, and business research developments. The design guidelines reinforce the Urban Village and small town image through the emphasis of architecture, landscaping, and hardscape elements at the pedestrian level.

The entire project site is interconnected by sidewalks along public streets, pedestrian and multi-use paths, and bike lanes on major public streets. Public plazas and gathering places in the residential and mixed-use areas are easily accessed by the pedestrian network to enhance the walking experience. A few minutes of walking will take residents and workers anywhere within the proposed project. A bus stop and shelter for the Gold Coast Transit Ventura Road route is located at the project entry intersection of Ventura Road and Beverly Drive. The pedestrian circulation plan is illustrated in Figure 3-2. For street design cross-section dimensions see Section 4.2, Figure 4-2 to Figure 4-10.

Various traffic calming methods shall be utilized in the planning area including “chokers” at local and collector street intersections with crosswalks, clearly defined to promote pedestrian movement between the neighborhoods. Roundabouts or traffic circles may be incorporated into local and collector streets and private drives to reduce traffic speeds, promote even traffic flows, and provide safe crossing locations for pedestrians. Landscape within the roundabouts aim to soften the visual impact of roadways and reduce the dominance of automobiles in the pedestrian context.

**Table 3-4
Development Standards Summary**

Zoning	R-2 Detached Cluster	R-2 Attached Condo	R-3	R-4	Mixed Use	C-2	BRP
Maximum Density	12	12	18	30	18	N/A	N/A
Maximum Height	30 ft. ¹	30 ft. ¹	45 ft. ¹	50 ft. ¹	45 ft. ¹	35 ft. ¹	35 ft.
Minimum Lot Area	2,500 sf	N/A	N/A	N/A	N/A	10,000 sf	20,000 sf
Minimum Lot Frontage	40 ft. ²	N/A	N/A	N/A	N/A	N/A	100 ft.
Minimum Lot Width	40 ft. ²	N/A	N/A	N/A	N/A	N/A	100 ft.
Minimum Lot Depth	50 ft. ²	N/A	N/A	N/A	100 ft.	N/A	100 ft.
Private Open Space	150 ft. ³	150 sf ³	100 sf ³	50 sf ³	50 sf ³	N/A	N/A

Note: Overall developed residential building footprint coverage of the Specific Plan cannot exceed 25% of the total residential area of the Specific Plan, calculated inclusive of streets, paved areas, parks, landscape, open space and private yards, in accordance with the Oxnard Airport Master Plan. Commercial and Industrial uses cannot exceed 50% building footprint coverage of the total Commercial and Industrial use areas.

Set-back Requirements

Front Building	10 ft.	10 ft.	10 ft.	15 ft.	0 ft.	10 ft.	30 ft.
Front Porch	10 ft.	10 ft.	N/A	N/A	N/A	N/A	N/A
Garage	18 ft. ⁴	3 ft.	3 ft.	3 ft.	N/A	N/A	N/A
Side	5 ft.	5 ft.	5 ft.	5 ft./story	5 ft./story	5 ft.	Equal to bldg height
Rear	5 ft.	5 ft.	5 ft.	5 ft./story	5 ft./story	15 ft.	20 ft.
Front-to-Front	25 ft.	25 ft.	25 ft.	30 ft.	25 ft.	N/A	N/A
Patio Cover	5 ft.	5 ft.	5 ft.	N/A	5 ft.	N/A	N/A

General Note: Allowed uses within all zoning classifications shall be consistent with those allowed or conditionally allowed by Chapter 16 of the City of Oxnard Municipal Code. All projects within the Specific Plan require Development Design Review. In addition, approval of a Special Use Permit shall be required for any conditionally allowed uses or any deviations from the Development Standards within the Specific Plan. Any deviations from the Residential Development Standards of this Specific Plan require approval of a Residential Planned Development (RPD) Permit as defined in City of Oxnard Municipal Code Division 12 Residential Planned Unit Developments. The Setback Requirements of this Specific Plan shall be applied as the minimum requirements for projects within the Specific Plan, unless modified by an Special Use Permit. Such conditions that concern numerical standards shall not increase or decrease by more than 25 percent the requirements that apply in the basic zone.

Footnotes:

1. Additional height allowed subject to approval of a Special Use Permit and conformance with FAA requirements.
2. Decreased dimension allowed subject to approval of a Special Use Permit.
3. Private Open Space defined as residential unit patio, porch and / or balcony with a six-foot by six-foot minimum dimension.
4. Garage set-back must be greater than the building set-back from street frontage property line.



FIGURE 3-2 PEDESTRIAN CIRCULATION



3.6.2 RESIDENTIAL DESIGN OBJECTIVES

The primary residential design objective is to use Urban Village design concepts and architecture to create effective and balanced pedestrian-oriented environments.

Residential uses make up approximately 50 percent of the Specific Plan area. Within the residential planning areas, three land use densities are allowed: Low-Medium, Medium, and Medium-High density. In accordance with the City's General Plan, these designations allow maximum densities of 12, 18, and 30 units per acre, respectively. Within each of these land use density neighborhoods, the design guidelines allow for several styles of architecture. The development guidelines for Low-Medium and Medium residential land uses require a minimum of three separate floor plans and four distinct exterior elevations for each neighborhood. These design variations promote visual and aesthetic interest within these residential communities. Medium-High residential land uses require that a single, uniform architectural style be implemented for each individual project; each project shall be architecturally distinct in this land use category. This approach will create a sense of unique identity and character for each multi-family attached development with the Specific Plan.

3.6.3 URBAN VILLAGE & MIXED-USE COMMERCIAL DESIGN OBJECTIVES

The primary urban village and mixed-use design objective is to ensure that the Specific Plan provides appropriately scaled commercial projects that serve the needs of full-time residents and daytime employees. In this context the term "mixed-use commercial"

applies to retail, office, and restaurant uses along Ventura Road and near the Ventura Road and Beverly Drive intersection. Diverse neighborhood retail uses and professional office space encourage a walkable "park once" strategy that further reduces dependency on automobiles within the project.

A central public plaza will serve as a community hub along Beverly Drive and will anchor the Urban Village. The plaza will be located westerly of the Ventura Road intersection to minimize roadway noise impacts. Buildings near the plaza shall activate the public realm by including outdoor dining and recreational activities to create a vibrant social and cultural character consistent with the Coastal California motif. The plaza area shall have a minimum dimension of 100' in any direction and include both fixed perimeter seating and a central multi-function area for neighborhood arts and cultural use. The public plaza will include an identity feature that will further define the space. A conceptual illustration for the plaza and typical proposed features are provided in Figure 3-3.

The mixed-use area will include creative, contemporary architectural references to "Traditional" or "Spanish Colonial Revival" building design as described in Section 3.7 of this Specific Plan. The buildings and garden spaces of this area shall be linked by sidewalks and landscaping that visually reduce the dominance of required parking within the site. Patios or courtyards shall be included within this area and clearly-defined pedestrian access throughout the mixed use area shall be provided. A portion of the commercial parking shall be provided in parking lots clustered away from public street view and with consideration for shared use by daytime and evening functions.



The mixed-use area may include residential units in either a vertical or horizontal mixed-use setting. The transition between land use types shall be emphasized by landscape and hardscape features that encourage interaction, yet provide adequate separation for all uses. Residential units shall include patios or private decks. Buildings in this area will not exceed three stories, in keeping with the moderate-intensity character of the site and the adjacent community.

3.6.4 BUSINESS RESEARCH PARK DESIGN OBJECTIVES

The Specific Plan proposes approximately 9.1 acres of land designated for business research park uses. These uses are located along the southern perimeter of the planning area and provide a buffer from industrial zoning and uses along the south side of Teal Club Road and the Oxnard Airport. This area will serve as an employment center and provide smaller, light industrial business spaces linked to the surrounding community by the sidewalk network and Class II bike lanes, both within the Specific Plan and on the perimeter streets.

The design objective of this business research area is to create a distinct sense of place and an extension of the walkable community context established throughout the rest of the Specific Plan area. Direct connection to bike lanes and easily accessible sidewalks encourage the interaction and creativity essential to a healthy workplace. Patios and small-scale open spaces shall be included within each industrial site at locations that encourage frequent use and allow for connection to the Specific Plan pedestrian and bike lane network. The design

consistency and attention to both landscape function and architectural form are intended to provide a clear identity for the industrial area while creating a valuable economic and job-creation resource for the City of Oxnard.

The land use ranges allowed for this area shall be consistent with the City of Oxnard Business Research Park (BRP) Zone Classification.

The architectural character of the business research area shall not be limited to the range of styles identified in the residential section of the Specific Plan, but shall allow for contemporary themes that can more effectively serve the needs and express the identities of the anticipated users.

The business research buildings shall incorporate both passive and active energy-saving technologies and design. Photovoltaic Solar Panels shall be required within this planning area. Buildings are to be located near the Teal Club Road frontage and a minimum of 40 percent of tenant parking is to be clustered in parking areas away from major public street view to the extent practical in site design. Up to 60 percent of required parking shall be allowed between Teal Club Road and the building frontage in private parking lots. Outdoor storage is not allowed with the BRP zone. Loading areas and other service functions shall be effectively screened from public view to enhance the visual character of the area. A 20-foot wide landscape buffer and 8-foot high slumpstone block wall separating the residential street from the BRP area shall be incorporated in to the site design as shown in Figure 3-4. Buildings in the BRP zone shall not exceed 35' in total height, including required mechanical equipment screening and are subject to any additional Oxnard Airport height and lighting limitations.

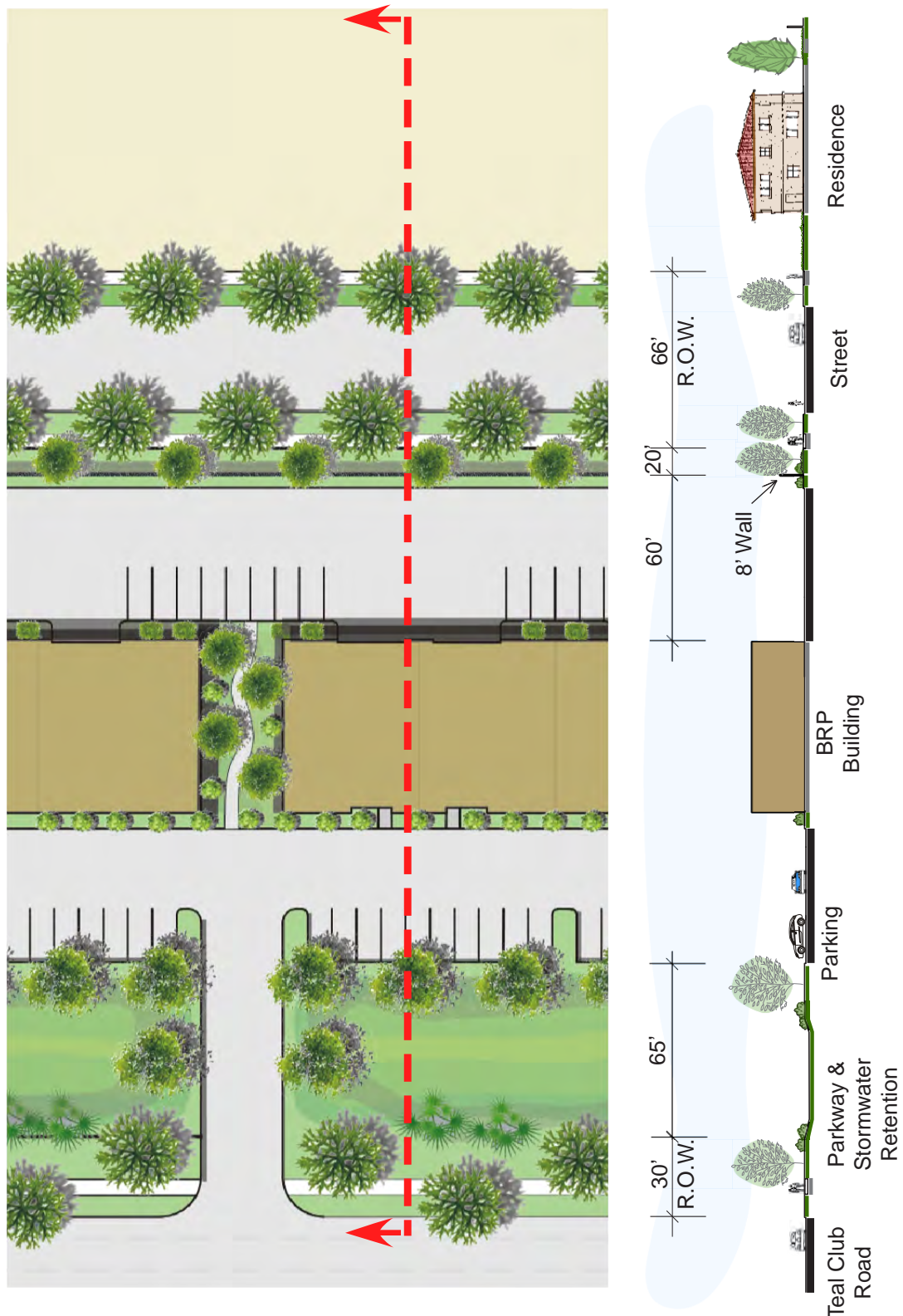
FIGURE 3-3 CENTRAL PUBLIC PLAZA



FIGURE 3-3 CENTRAL PUBLIC PLAZA



FIGURE 3-4 BRP LANDSCAPE BUFFER





3.6.5 OPEN SPACE DESIGN OBJECTIVES

The design objective of the Specific Plan open space areas is to ensure an integrated plan addressing recreational and aesthetic issues. All run-off from the project will be captured and treated to the standards of the Environmental Protection Agency and Regional Water Quality Control Board through a series of bio-swales, filtration ponds, and other vegetated treatment systems. Plant materials selected for the drainage network shall consist of native species and selected adaptive plant materials appropriate for both habitat and visual enhancement for either upland, transitional, or wetland locations. Approximately 5.4 acres of Stormwater Treatment and Detention Basins are proposed within the overall Specific Plan area.

Additional open space within the plan area includes approximately 17.7 acres of parks and greenbelts. These sites are intended to provide for the recreational needs of the residents of the plan area in an interconnected network of paths, trails, and sidewalks.

The neighborhood park and recreation facilities shall be designed and programmed in cooperation with the Oxnard Recreation and Community Services Department to ensure that the widest possible range of neighborhood-compatible outdoor activities are provided within the planning area.

3.7 DESIGN GUIDELINES

3.7.1 ARCHITECTURAL STYLES

This section provides the overall design guidelines for residential and other land uses within this Specific Plan area. Architecture is a key component of the appearance of the Specific Plan area. The design guidelines outlined in the following sections provide general design criteria and promote visual compatibility while allowing for individuality and architectural diversity. Four architectural styles form the basis for these design guidelines: Craftsman, Traditional, Spanish Colonial Revival, and Monterey (see Figures 3.5-3.8). Creative, contemporary interpretation of these styles is strongly encouraged. All homes shall provide solar panels and be pre-wired to accommodate electric vehicle plug-in portals.

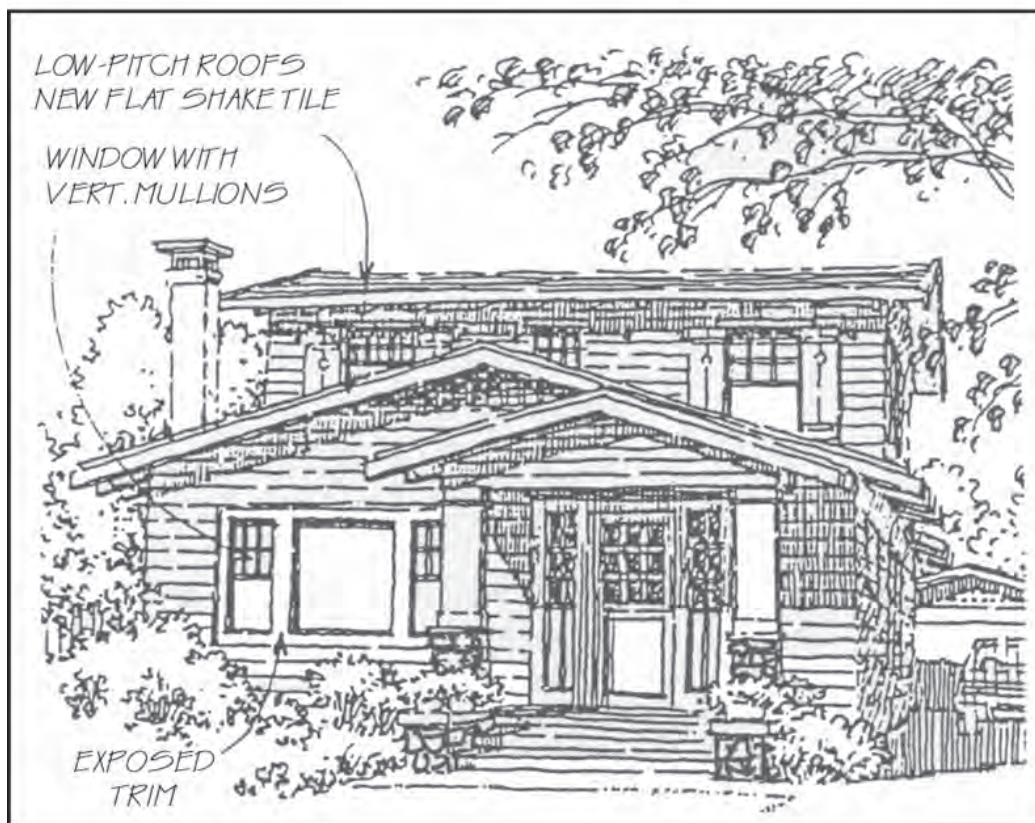
The architectural styles chosen have remained popular over time and are consistent with the prevailing history and design character of the region and the nearby Oxnard Historic District. They are intended to provide direction to the architect, while allowing latitude in the interpretation of the style. The styles represented are not intended to be prescriptive, but are intended to provide inspiration and encouragement. Additional architectural styles may be considered, subject to approval by the Development Services Director. Contemporary themed architectural styles are recommended in the Specific Plan Business Research use planning areas.

Craftsman - Figure 3-5

Unique characteristics of the Craftsman style include rustic texture of building materials, broad overhangs with exposed rafter tails at the eaves, and often extensive pergolas and trellises over large porches at the lower level. Roofs are low-pitched and gabled with wide unenclosed eave overhangs. Windows are often combined into horizontal groupings and are surrounded by wide and carefully detailed wood or other appropriate trim.

Elements	Description
<i>Roof</i>	<ul style="list-style-type: none"> <input type="checkbox"/> One and three-story structures (four-story allowed in Medium-High Density). <input type="checkbox"/> Low-pitched gable roofs with wide unenclosed eave overhangs. <input type="checkbox"/> Multiple roof planes. <input type="checkbox"/> Decorative beams or braces under gable. <input type="checkbox"/> Exposed, profiled rafter tails or fascia board set eaves, and simple wide bargeboards with trim at rakes (no stucco soffits). <input type="checkbox"/> Flat concrete or slate tile roofs.
<i>Windows</i>	<ul style="list-style-type: none"> <input type="checkbox"/> Vertical and square windows combined into horizontal groupings. <input type="checkbox"/> Divided lites in upper sashes of single- or double-hung windows. <input type="checkbox"/> Surrounded by wide and carefully detailed wood or other appropriate trim. <input type="checkbox"/> Window dormers with gable or shed roof.
<i>Wall Materials</i>	<ul style="list-style-type: none"> <input type="checkbox"/> Wide, horizontal wood siding or smooth stucco. <input type="checkbox"/> Frequent use of natural or simulated cobblestone and river rock. <input type="checkbox"/> Occasional use of brick at base of wall or porch. <input type="checkbox"/> Rubble stone faux chimneys. <input type="checkbox"/> Use of natural materials such as redwood, tile, and stone. <input type="checkbox"/> Exposed wooden balconies. <input type="checkbox"/> Trellis porch or porte-cochere.
<i>Architectural Elements</i>	<ul style="list-style-type: none"> <input type="checkbox"/> Tapered wood posts set on rectangular or tapered masonry piers. <input type="checkbox"/> Exposed wood beams and brackets, profiled at the ends. <input type="checkbox"/> Extensive use of gabled porches. <input type="checkbox"/> Band sawed and pierced ornamentation in the wood railings.
<i>Colors</i>	<ul style="list-style-type: none"> <input type="checkbox"/> Brown rich contrasting accents. <input type="checkbox"/> Lighter whites, grays, flat blues, earthy greens, and yellows.

Craftsman - Figure 3-5



Traditional - Figure 3-6

Unique features of the Traditional style draw on forms and details from the early ancestral homes along the Atlantic seaboard of Colonial America. Creative touches of wood and horizontal siding define the Traditional style, which is identified by simple one and two story rectangular boxes with a primary front to back side-gabled roof. Hip roofs tend to occur at the first story roof lines associated with porch elements.

Elements	Description
<i>Roof</i>	<ul style="list-style-type: none"> ❑ One- or two-story except as deemed appropriate to specific design. ❑ Flat concrete tile or composition shingle tile . ❑ Roof pitches are steeper normally ranging from 5:12 to 7:12. First story porch roofs can have lower pitches of 3:12 to 4:12. ❑ Overhangs are typically shallow; 6” – 12” rakes with 12’ – 18” eaves, sometimes with the use of double trim fascia/barge boards.
<i>Windows</i>	<ul style="list-style-type: none"> ❑ Single hung rectangular multi-paned windows frequently arranged in pairs or stacked at first and second floors. ❑ Bay windows. ❑ Typically louvered shutters at primary windows – sized to match window openings.
<i>Wall Materials</i>	<ul style="list-style-type: none"> ❑ Stucco. ❑ Both horizontal and vertical accent siding is used – board and batten, clapboard and lap siding. ❑ Brick or stone is used mostly as a wainscot at the first story or on built-up column/post bases.
<i>Architectural Elements</i>	<ul style="list-style-type: none"> ❑ Accentuated front entry door with pediment and columns or covered front porch. ❑ Decorative vents or trusses at gable ends. ❑ Wood columns/posts and railing with vertical pickets. ❑ Simple garage doors. ❑ Occasional use of dormer windows. ❑ Wood outlookers.
<i>Colors</i>	<ul style="list-style-type: none"> ❑ White or a variety of brighter, bolder theme colors. ❑ Typically white/off white trim and fascia/barge board. ❑ Body siding colors should match the stucco color, however areas of accent siding should be contrasting darker shades or white. ❑ Deeper, darker shutter and accent colors.



Traditional - Figure 3-6



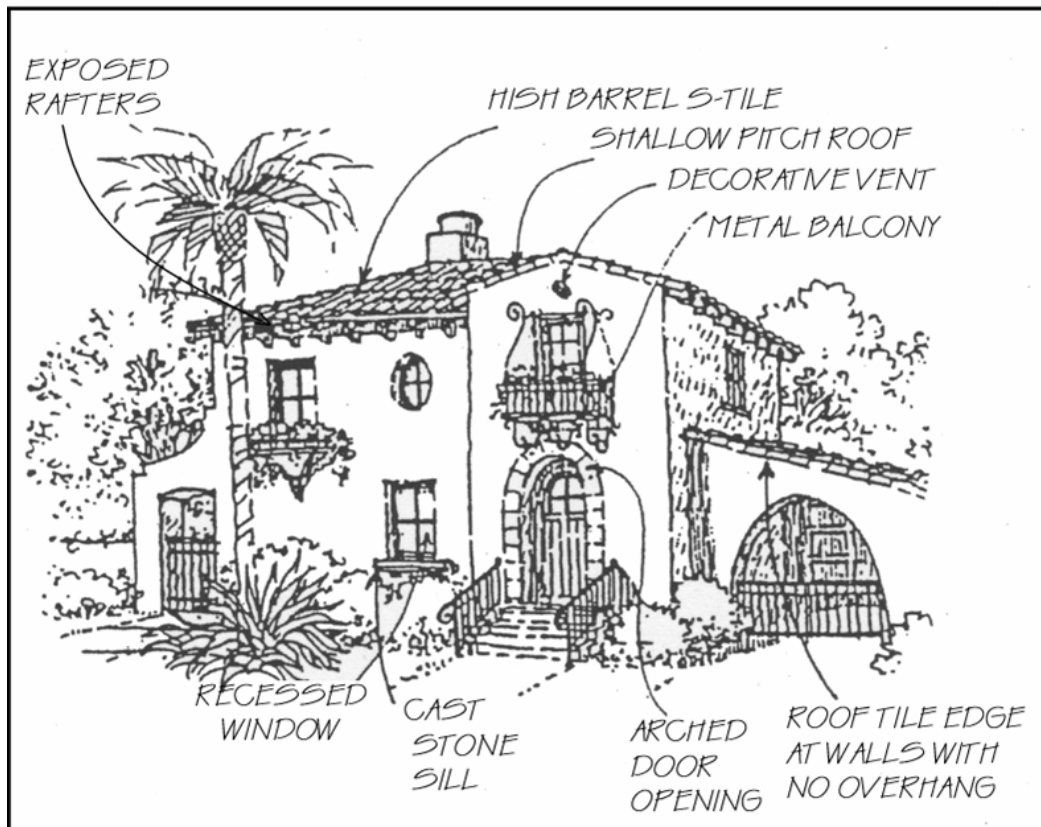
Spanish Colonial Revival - Figure 3-7

Unique features of the Spanish Colonial Revival style include red tile hipped roofs, ornamental ironwork over windows or on balconies, arcaded porches, ornate low-relief carvings highlighting arches, columns, window surrounds, and parapets. Stone or brick exterior walls often are left exposed or finished in plaster. Windows can be either straight or arched.

Elements	Description
<i>Roofs</i>	<ul style="list-style-type: none"> <input type="checkbox"/> One and three-story structures (four-story allowed in Medium-High Density). <input type="checkbox"/> Low-pitched gabled roof with little or no overhang. <input type="checkbox"/> Simple hip, gable, and shed forms. <input type="checkbox"/> Exposed rafter tails or profiled cornice at eaves. <input type="checkbox"/> No overhang at gables, with tile wrapped over face of wall, with or without profiled cornice and/or stucco soffits. <input type="checkbox"/> High barrel concrete “S” tile in red colors, or two-piece/high-profile “S” clay tile.
<i>Windows</i>	<ul style="list-style-type: none"> <input type="checkbox"/> Recessed feature windows at façades. <input type="checkbox"/> Flat, arched, segmental, or half round window heads. <input type="checkbox"/> No trim typically at heads or jambs of typical openings. <input type="checkbox"/> Stone or simulated cast stone, pre-cast concrete or stucco over foam at accent openings, or at sills of typical openings.
<i>Wall Materials</i>	<ul style="list-style-type: none"> <input type="checkbox"/> Smooth finish stucco (no rougher than sand or light lace finish). <input type="checkbox"/> Cut or simulated stone accents. <input type="checkbox"/> Clay or polyurethane/foam material made to faithfully simulated clay pipe scuppers and vents. <input type="checkbox"/> Shutters at windows accent windows.
<i>Architectural Elements</i>	<ul style="list-style-type: none"> <input type="checkbox"/> Heavy exposed beams. <input type="checkbox"/> Iron, steel bar block simulating wrought iron, or wood balconies and rails. <input type="checkbox"/> Masonry, stucco, or wood column and balustrades. <input type="checkbox"/> Arcades and trellises. <input type="checkbox"/> Decorative tile insets; decorative wrought iron railings, window treatments, or balconies. <input type="checkbox"/> Low stucco wall at frontage, enclosing forecourt.
<i>Colors</i>	<ul style="list-style-type: none"> <input type="checkbox"/> White and off-white walls. <input type="checkbox"/> Dark brown wood. <input type="checkbox"/> Vibrant paint and tile accents.



Spanish Colonial Revival - Figure 3-7



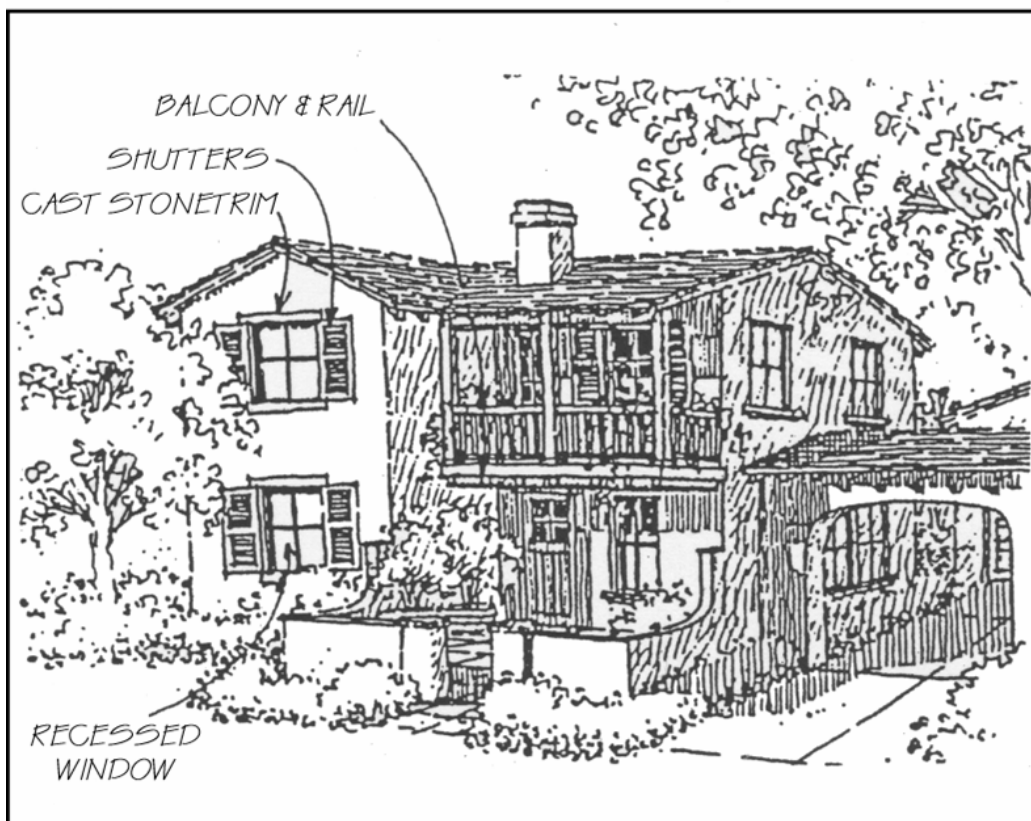
Monterey - Figure 3-8

Unique characteristics of the Monterey style include low-pitched roofs with flat concrete tile, usually in tones of browns and brownish terra cotta. The Monterey typically has a prominent second floor covered wood balcony or covered front porch at the façade. Wall surfaces are always stucco and the forms are usually asymmetrical.

Elements	Description
<i>Roofs</i>	<ul style="list-style-type: none"> <input type="checkbox"/> One and three-story structures (four-story allowed in Medium-High Density). <input type="checkbox"/> Low-pitched gabled roof with little or no overhang. <input type="checkbox"/> Simple hip, gable, and shed forms. <input type="checkbox"/> Exposed rafter tails or profiled cornice at eaves. <input type="checkbox"/> No overhang at gables, with tile wrapped over face of wall, with or without profiled cornice and/or stucco soffits. <input type="checkbox"/> High barrel concrete “S” tile in red colors, or two-piece/high-profile “S” clay tile.
<i>Windows</i>	<ul style="list-style-type: none"> <input type="checkbox"/> Recessed feature windows at façades. <input type="checkbox"/> Flat, arched, segmental, or half round window heads. <input type="checkbox"/> No trim typically at heads or jambs of typical openings. <input type="checkbox"/> Stone or simulated cast stone, pre-cast concrete or stucco over foam at accent openings, or at sills of typical openings.
<i>Wall Materials</i>	<ul style="list-style-type: none"> <input type="checkbox"/> Smooth finish stucco (no rougher than sand or light lace finish). <input type="checkbox"/> Cut or simulated stone accents. <input type="checkbox"/> Clay or polyurethane/foam material made to faithfully simulated clay pipe scuppers and vents. <input type="checkbox"/> Shutters at windows accent windows.
<i>Architectural Elements</i>	<ul style="list-style-type: none"> <input type="checkbox"/> Heavy exposed beams. <input type="checkbox"/> Iron, steel bar block simulating wrought iron, or wood balconies and rails. <input type="checkbox"/> Masonry, stucco, or wood column and balustrades. <input type="checkbox"/> Arcades and trellises. <input type="checkbox"/> Decorative tile insets; decorative wrought iron railings, window treatments, or balconies. <input type="checkbox"/> Low stucco wall at frontage, enclosing forecourt.



Monterey - Figure 3-8



3.7.2 RESIDENTIAL DEVELOPMENT

Residential uses include various densities ranging from R-2 (maximum 7 to 12 dwelling units per acre), to R-3 (maximum 12 to 18 dwelling units per acre), to R-4 (18 to 30 units per acre). A fundamental planning principle of this Specific Plan is to incorporate a range of development densities located within close proximity. Higher densities will be designed as clustered neighborhoods, with resident parking and automobile access from an interior parking court which is accessed off of secondary streets. This design reduces the number of curb cuts and automobile activity on the main streets, improving automobile circulation and maintaining pedestrian friendly activity. These higher density clusters will be located along the main circulation arteries or near employment centers and retail services. Less dense residential development will be oriented away from the commercial and automobile activity. This type of clustered neighborhood design with access and orientation to the street is

consistent with the Traditional Neighborhood Design Concept proposed for this Specific Plan.

The following Design Guidelines for this Specific Plan are consistent with both the City of Oxnard Zoning Code and the recommended architectural styles.

General Design Guidelines for Residential Uses

The following guidelines apply to all residential development within the Teal Club Specific Plan area.

Residential	General Design Guidelines for Residential Uses	Table 3-4
<i>Site Planning</i>	<ul style="list-style-type: none"> □ Orientation of buildings to the street is a fundamental objective of this Specific Plan. In multi-family residential areas, one of the prevalent features shall be an attractive internal pedestrian-oriented circulation system. □ Street configuration is designed to maintain the majority of automobile traffic on main circulation arteries and limit the volume of automobile traffic on secondary streets. □ Neighborhood design along main travel arteries should be clustered with interior parking courts, which access off of secondary streets, thus limiting the number and frequency of curb cuts along the main street. □ Parking shall be provided for permitted uses in accordance with the provisions of the Zoning Code. Required parking for residences shall be located and designed in conformance with the development standards and the design guidelines. 	



Residential	General Design Guidelines for Residential Uses	Table 3-4 continued
<i>Architectural Details</i>	<ul style="list-style-type: none"> □ These guidelines define a range of materials, elements, and details required for residential units and site improvements within the Specific Plan area and apply to all architectural styles recommended for residential land uses (see Section 3.7.1 for recommended architectural styles). 	
<i>Roofs and Massing</i>	<ul style="list-style-type: none"> □ Allowable building forms shall be three-and four story for Medium-High, two-and three story massing for Medium Density residential and two-story for the Low-Medium density residential to be accompanied by taller appropriate details, as appropriate for the particular architectural style. □ Overhanging eaves should be finished with exposed rafter tails or a cornice, as appropriate for the selected architectural style. The minimum dimension of exposed rafter tails should be three inches. □ Skylights should be flat (non-bubble) only. □ Gutters should be painted galvanized steel or painted aluminum. Allowed gutter shapes are half round and ogee. □ Roofing materials should be concrete barrel or flat tile or high-quality composite materials. □ The roofing material for each house or building should be consistent with the selected architectural style, as specified in the architectural styles (Section 3.7.1) of these guidelines. 	
<i>Doors and Windows</i>	<ul style="list-style-type: none"> □ Exterior doors and windows should be made of wood, vinyl clad wood, vinyl, or metal. □ Window glass should be clear glass with not more than 10 percent daylight reduction (tinting), and should not be reflective (mirrored). □ Windows may have the following accessories: shutters sized to match the openings, canvas awnings appropriate to the architectural style, and planter boxes supported by visible and decorative brackets. 	

Residential	General Design Guidelines for Residential Uses Table 3-4 continued
<i>Doors and Windows (cont.)</i>	<ul style="list-style-type: none"> □ Windows and doors may be trimmed in painted wood, brick, stone, or cast stone. □ Windows may be single-hung, double-hung, hinged casement, or slider. Windows on main façades should be consistent with the selected architectural style. □ Façades adjacent to or visible from the street should be fenestrated with windows and doors, identifying a principal entry to the building where practical.
<i>Walls and Fences</i>	<ul style="list-style-type: none"> □ Building walls should be made of or finished in smooth or sand finish stucco, painted wood siding, brick, stone, or approved composite materials faithfully simulating these natural materials appropriate to the architectural style. □ Perimeter garden walls should be made of concrete sand - colored slump masonry material. □ Painted wood picket fences are allowed at the frontages of lots in the Low-Medium residential areas as long as they match the architectural style of the main structure. These fences should be a maximum of 36 inches in height and all fences shall be set back at least three feet from back of sidewalk with landscape between the sidewalk and fence. □ Rear yard walls visible from a street should be made of sand-colored slump block masonry. Interior yard walls and gates may be vinyl. □ Such walls should be no more than six feet in height, except where noise sources or adjacent grade differences require higher walls.
<i>Mechanical and Electrical Equipment</i>	<ul style="list-style-type: none"> □ All mechanical and electrical equipment—including but not limited to air-conditioning units and antennas—whether roof-mounted, ground-mounted, or otherwise, should be screened from street view with construction materials and a design that is consistent with the architectural style of the main structure. An exception shall be made for flat roof-mounted solar panels.
<i>Colors</i>	<ul style="list-style-type: none"> □ Roofs: Fireproof Materials appropriate to the architecture style of the main structure in color and texture. □ Walls: Natural materials or composite materials faithfully replicating the color and texture of the natural material, appropriate to the architectural style of the main structure. □ Accents: Traditional and historically correct colors to complement the selected architectural style.



R-2 Density Residential Guidelines

This residential density creates a moderate intensity development and provides for residential development of 7-12 units per acre in the form of two-story buildings, courtyard housing, condominiums, townhouses or detached two-story single-family houses. Buildings are to be designed in a manner that orients the main façades to the adjacent street and takes advantage of the associated circulation system. On-site parking shall be located primarily in street-facing or clustered garages. On-street parking shall be encouraged through site design. Minimal front yard setbacks will be used along main circulation arteries. Private outdoor space in the form of balconies, patios, and porches is required. Buildings should have definitive entries and should function to reduce the physical scale of the buildings through the use of pedestrian scale design elements. Development standards specified within the Development Standards (Table 3-4) of this Specific Plan shall be utilized. Additionally, the following Development Guidelines shall apply.



Residential	R-2 Density Development Guidelines	Table 3-5
	<ul style="list-style-type: none"> ❑ Two-story maximum building height of 30 feet, or up to 35 feet with an approved Residential Planned Development Permit (RPD). ❑ A minimum of three variations of floor plans and three distinct exterior elevations shall be required. ❑ Lot sizes shall be per the Teal Club Specific Plan (Table 3-4). ❑ Front yard setbacks to porches or building shall be a minimum of 10 feet from adjacent street right of way, with a 2-foot minimum front setback variation between adjacent residences. ❑ Side and rear setbacks shall be a minimum of five feet, subject to site plan approval. ❑ Parking garages shall not be oriented to the adjacent arterial or collector street and shall be accessed through a private drive aisle or parking court where feasible. Garages facing local streets shall be set back further than the porch or main structure. ❑ The minimum parking requirements shall be per the City of Oxnard Zoning Ordinance. Of the spaces required, only guest parking spaces are allowed as “on-street” parking. ❑ Craftsman, Traditional, Spanish Colonial Revival, and Monterey architectural styles and contemporary interpretations are appropriate for Low-Medium Density Development. Trash receptacles, electrical transformers, and other service elements shall be located in areas where parking is permitted and shall be screened from public view within the allowable standards of public utility agency requirements. ❑ All mechanical and electrical equipment—including but not limited to air-conditioning units, and antennas—whether roof-mounted, ground-mounted, or otherwise, shall be screened from public view, except as limited by other agency regulations. An exception shall be made for flat-mounted solar panels. ❑ A minimum of two fully enclosed garage spaces are required for each detached unit. 	



R-3 and R-4 Residential Design Guidelines

This residential category provides for residential development from 12 to 30 units per acre in the form of two- and three-story condominium buildings, courtyard housing, or townhouses as well as three- and four-story apartment buildings. Development standards per the City of Oxnard Zoning Ordinance for the R-3 and R-4 Zone Classifications shall apply, except as specified within the Development Standards (Table 3-4) of this Specific Plan. This density provides a moderate transition from arterial roadways and nearby commercial and industrial development. Buildings are to be designed in a manner that orients the main façades to the adjacent street and takes advantage of the associated circulation system. Resident parking shall be located primarily behind buildings or otherwise screened from street view. Guest parking may be accommodated in a combination of designated on-site spaces and on-street parking along the perimeter of a multi-family residential parcel. Clustered design is encouraged. Minimal front yard setbacks will be used along main internal circulation roadways. Private outdoor space in the form of balconies, patios, or porches is required. Buildings should have definitive entries and should function to reduce the physical scale of the buildings through the use of pedestrian scale design elements.

Pedestrian or bike access will be an integral part of the site design. The adjacent pedestrian oriented circulation system can be used as a secondary recreation area as it relates to providing residents with an enjoyable route to walk, jog, bike, etc. The following Development Guidelines shall apply.



R-3 AND R-4 DENSITY RESIDENTIAL

Residential	R-3 and R-4 Density Development Guidelines
	<ul style="list-style-type: none"> ❑ R-3 shall have a three-story maximum building height of 45 feet, and R-4 shall have a four-story maximum building height of 50 feet, with any exceptions requiring an approved Residential Planned Development Permit (RPD) subject to FAA restrictions and in accordance with zoning standards. ❑ Lot sizes shall be per the Teal Club Specific Plan (Table 3-4) and comply with Subdivision Map Act standards for attached and multi-family dwelling units. ❑ Front yard setbacks along circulation arteries shall be 15 feet as measured from the adjacent right of way. On corner lots the front setback applies to both façades facing an adjacent roadway. ❑ Side and rear setbacks shall be determined by the individual Planned Development Permit of each project and may be less than those established in the Oxnard Zoning Code. Minimum building separation shall be 5 feet per story of the taller of two adjacent buildings. (Example: 15 foot building separation between a three-story building and a two-story building.) This shall apply to buildings located on either the same parcel or adjoining separate parcels. ❑ Parking areas shall not be located between the building and the adjacent street and shall be configured as “parking courts.” Any parking areas visible from a street frontage shall be screened from view by a low wall or by continuous landscaped shrubbery. ❑ The minimum parking requirements shall be per the City of Oxnard Zoning Ordinance. Of the spaces required, only guest parking spaces are allowed as “on-street” parking. Tandem garage parking shall be allowed in R-3 for-sale residential projects and included in parking count totals. Tandem spaces shall be assigned to the same unit. ❑ Trash receptacles, electrical transformers, and other service elements shall be located in areas where parking is permitted and shall be screened from public view within the allowable standards of public utility agency requirements. ❑ All mechanical and electrical equipment—including but not limited to air-conditioning units, and antennas—whether roof-mounted, ground-mounted, or otherwise, shall be screened from public view, except as limited by other agency regulations. An exception shall be made for flat roof-mounted solar panels. ❑ Craftsman, Traditional, Monterey and Spanish Colonial Revival architectural styles and contemporary interpretations are appropriate for Medium and Medium High Density Development.



3.7.3 COMMERCIAL & MIXED-USE DEVELOPMENT (C-2)

The commercial and mixed-use land use components of the Specific Plan occur along Ventura Road and Beverly Drive. Mixed-use properties are a unique development type in that they combine retail and office commercial uses in a single site along with residential uses in suitable locations. The typical mixed-use design primarily has retail or office uses located on the first-floor street or public plaza frontage, while residential uses occupy the upper floors. Mixed-use development is consistent with the Traditional Neighborhood Design form proposed for this Specific Plan. Mixed-use development is conducive to the small town, pedestrian-oriented design elements proposed in this plan.

The mixed-use area is envisioned as a gathering place consisting of a combination of single buildings and building groups with landscape and hardscape details as a focal point. The ambiance of the mixed-use zones shall be open-air, inviting, and accessible to the public.

The most desirable design will utilize combinations of construction materials and accent features such as brick, stucco, wood, or stone. Heights will vary from one to three-stories and façades will be modulated to create the desired “village” environment. Contemporary interpretation of the Traditional or Spanish Colonial Revival theme will build upon this ambiance and allow architectural variety in the mixed-use area.

The mixed use development will complement the residential development in the rest of the Specific Plan area. Visually and physically,

where two planning areas abut, there shall be a smooth transition so as not to detract from the overall design. This smooth transition is accomplished through the continuation of the adjacent mixed-use architectural styles, the use of consistent landscape and hardscape materials, the pedestrian-oriented street design, and the street intersection corner landscape open space design.

General Design Guidelines for Commercial & Mixed-Use (C-2)

The following Design Guidelines for the mixed-use development zones are consistent with the City of Oxnard General Commercial Zone (C-2) standards, except as specified within this Specific Plan, and are intended to highlight and expand on the uniqueness of these highly visible public spaces.



MIXED-USE

Commercial & Mixed Use	General Design Guidelines for Mixed Use & Commercial (C-2)
<i>Site Planning</i>	<ul style="list-style-type: none"> ❑ Buildings should be aligned along a plaza or sidewalk. Outdoor cafes and seating areas are encouraged. ❑ Main entrances should have an architectural definition, such as an awning, recessed niche, three dimensional feature, or building projection. ❑ A combination of hardscape and landscape is required from the street or parking lots to the building wall on every setback area.
<i>Architectural Character</i>	<ul style="list-style-type: none"> ❑ C-2 buildings shall have a two-story maximum building height of 35 feet, and Mixed Use shall have a three story maximum building height of 45 feet except with an approved Design Development Review Permit. ❑ Buildings should maintain architectural articulation and visual quality on all sides. ❑ Front façades for mixed-use buildings should be designed to create an individual identity to each vertical module or unit. ❑ Storefront or display windows shall encompass a minimum of 40 percent and a maximum of 70 percent of the building facade. ❑ All mechanical and electrical equipment including but not limited to air conditioning units, and antennas (whether roof-mounted or otherwise), should be screened from public view. ❑ The mixed-use area shall allow a wider range of colors, materials, and forms to encourage a creative interpretation of the Traditional or Spanish Colonial Revival styles.
<i>Parking and Vehicular Access</i>	<ul style="list-style-type: none"> ❑ The Commercial frontage along Ventura Road shall allow for one driveway curb cut between Doris Avenue and Beverly Drive. A visible parking lot is allowed on the Ventura Road frontage. ❑ Parking access, driveway design and parking areas shall provide clear path of pedestrian travel to store fronts. ❑ Parking shall be provided for permitted uses in accordance with the provisions of the Zoning Code. ❑ Nonresidential parking standards remain consistent with the City of Oxnard Zoning Ordinance; however, a shared parking agreement may be developed where practical and enforceable. ❑ Service and loading facilities must be screened from view from public streets, open spaces, and other sensitive uses. ❑ Trash receptacles, electrical transformers, and other service elements should be located in areas where parking is permitted and should be screened from public view by walls with matching architectural elements as those of the buildings they serve.



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3.7.4 BUSINESS RESEARCH PARK DEVELOPMENT (BRP)

Industrial uses within the Specific Plan are intended to promote a balance of jobs and housing within close proximity, reducing the dependence on automobiles. The Specific Plan industrial use areas are located immediately north of and adjacent to existing industrial zoning and provide a logical extension of this land use type. The intent of the Specific Plan industrial area is to provide a Business Research Park (BRP) zoned location for new or expanded manufacturing, research, professional, and high technology uses consistent with the economic development goals of the City of Oxnard. The walkable community planning principle of the Specific Plan places an emphasis on high

quality of life for the daytime workforce with direct access to trails, greenbelts, and parks. Commercial services including restaurants and shopping are planned within a ten minute walking distance of the majority of industrial parcels.

The following Development Guidelines are consistent with the Business Research Park (BRP) Zone, except as specified within this Specific Plan and are consistent with the recommended architectural styles.

General Design Guidelines for BRP Uses

The following guidelines apply to all industrial development within the Teal Club Specific Plan area.

Industrial	General Design Guidelines for BRP
<i>Site Planning</i>	<ul style="list-style-type: none"> □ Buildings should be located near Teal Club Road with at least 40 percent of parking and all service and loading activities located away from street view. The remainder of parking may be on the building street frontage side. □ A combination of hardscape and landscape is required from the street to the building wall on every setback area. □ Main entrances should be prominent and easily identified and accessible from clearly defined guest parking areas. □ All on-site lighting shall utilize light standards and devices that provide down-lighting that is shielded from abutting public streets, residential areas, or adjoining properties. □ Lot sizes shall be a minimum of 20,000 square feet.
<i>Architectural Character</i>	<ul style="list-style-type: none"> □ Two-story maximum building height of 35 feet. □ Architectural styles shall include contemporary themes unique to the industrial area or interpretations of the Specific Plan styles. □ Buildings shall maintain architectural articulation on all sides. □ Buildings shall not exceed two stories in height and are subject to Oxnard Airport height and lighting imitations. □ Building facades must have at least one break in wall plane of at least three feet in depth every 100 feet in length. □ Flat roofs shall be allowed with adequate mechanical space parapet screening.



Industrial	General Design Guidelines for BRP
<i>Architectural Character (cont.)</i>	<ul style="list-style-type: none"> □ All mechanical and electrical equipment including but not limited to air conditioning units and antennas (whether roof-mounted, ground-mounted or otherwise) should be screened from public view. An exception is made for solar panels which are integrated into the design of a building. □ Roofs shall be designed to accommodate solar panels. □ Tinted or reflective glass may be allowed, appropriate to the architectural style. □ The industrial use area shall allow a range of colors, materials, and forms to encourage architectural diversity within the limits of the style selected for an individual building or buildings. The decision-makers shall have authority on appropriateness of colors through the discretionary permit process.
<i>Walls and Fences</i>	<ul style="list-style-type: none"> □ Walls and fences are to be made of sand-colored slump masonry material. □ Walls shall be utilized to screen all permitted outdoor activities completely from offsite public view. □ The required height for any BRP zone wall or fence is eight feet, except with a discretionary permit which may allow up to ten feet in height. □ All BRP public-facing wall surfaces shall be planted with approved fast-growing vines and trellis material to prevent graffiti. □ Outdoor storage shall be subject to a Special Use Permit and shall not exceed the height of the perimeter screen wall.
<i>Parking and Vehicular Access (cont.)</i>	<ul style="list-style-type: none"> □ No loading areas may be located where they can be viewed directly from any adjacent or nearby residential areas. Loading areas are to be screened from view by walls or fences. □ A 20-foot wide landscape buffer and wall shall be incorporated into the site design separating the residential street from the BRP area (as shown in Fig 3-4). □ Trash receptacles, electrical transformers, and other service elements should be located in areas where parking is permitted and should be screened from public view by walls matching architectural elements of the buildings they serve. □ Trash enclosures shall include hose bibs and be maintained in a clean and odor-free condition. □ Industrial and office parking standards remain consistent with the City of Oxnard Zoning Ordinance; however, a shared parking agreement may be developed where appropriate, practical, and enforceable. □ Guest parking shall be provided at clearly identifiable locations near the entry to a building. □ No on-street loading is allowed in the industrial areas of the Specific Plan.

3.7.5 PUBLIC PARKS & HOA RECREATION AREAS

The Specific Plan provides for a range of active and passive park spaces with varied uses and amenities. The combination of private homeowners' association facilities and publicly owned and maintained parks are intended to meet the needs of the plan area residents and business employees as well as provide public benefit to the entire Oxnard community.

The intent of the park and open space plan is to ensure that all residents and employees are within a ten-minute walk of usable recreational amenities, encouraging an active healthy lifestyle. There are two public parks in the plan area: the Phase 1 and 2 portions of the ten-acre centrally-located Neighborhood Park and the 7.3 acre Phase 2 southwest area park. The central east-west greenbelt creates a visible connection for public gathering, organized recreation, walking, and biking.

Both public parks will be owned and maintained by the City of Oxnard and serve a portion of the recreational needs of the plan area and the existing nearby neighborhoods. Maintenance funding shall be provided by a Maintenance Community Facilities District (CFD) or other city-approved funding mechanism. Turf play areas, playground equipment, bathrooms, and picnic tables will be incorporated into the parks, subject to final design approval by the City of Oxnard.

HOA Private Recreation Areas

HOA-maintained private recreation areas constructed by developers and maintained by Property Owners' or Homeowners' Associations are allowed. These private spaces provide for a portion of recreational



needs and are available for use only by the residents and guests of the plan area or a sub-area of the Specific Plan.

All park and private recreation landscaping will be irrigated with City of Oxnard water and comply with the landscape guidelines of the Specific Plan. The park plans shall also comply with the Recreation Element of the General Plan. The following guidelines shall be considered when planning and designing the Public Parks and HOA-owned facilities within the Teal Club Specific Plan area.



Parks	General Design Guidelines for Parks
<i>Neighborhood Parks</i>	<ul style="list-style-type: none"> ❑ Park shall be operated and maintained by City of Oxnard Recreation and Community Services Department. All costs of maintenance of the parks shall be borne by the Specific Plan developments under a financing mechanism as defined within the Phase 1 and Phase 2 Development Agreements. ❑ No more than 50 percent of the total park area shall be reserved for turf and shall also provide areas for passive recreational space. ❑ A portion of the park area shall be designed as active recreational space with related equipment that shall include play structures and/or a tot play area. ❑ Organized sports fields may be considered for the turf areas. Field lighting shall be allowed subject to Oxnard Airport approval. Lighting for pedestrian and parking areas shall be designed to the highest available light cut-off standards due to the proximity to the Oxnard Airport and nearby residences. Pedestrian and parking lot pole lighting shall not exceed 20 feet in height. ❑ Each park shall include at least six picnic tables and appropriate apparatus; group picnic facilities may be allowed at this location and could include one shelter structure. ❑ Park benches, drinking fountains and trash receptacles will be included at appropriate locations. ❑ Restroom facilities shall be provided, subject to City approval. ❑ The park architectural character will be consistent with the theme of the allowed Specific Plan architectural styles. ❑ Landscape area will include clusters of trees dispersed throughout the park area. ❑ Park landscape will be designed to promote off-site visibility and public safety for policing and neighborhood awareness throughout the park. ❑ The roadway edges of the park should incorporate the adjacent streetscape character. ❑ No more than 15 percent of the park will be utilized for parking lots. On-street perimeter parking is allowed. Overnight parking is prohibited. ❑ Field lighting shall be designed to the highest available light cut-off standards to shield future adjacent homes from light and glare. Field lighting and hours of operation shall be managed by the City Recreational Services Department. ❑ All lighting shall comply with the City of Oxnard lighting requirements.

Parks	General Design Guidelines for Parks and Recreation Areas
<i>HOA Facilities</i>	<ul style="list-style-type: none"> ❑ Facilities shall be operated, funded and maintained by a private Property Owners' Association, or as required by an applicable Development Agreement. ❑ A maximum of 25 percent of the total overall HOA recreation and greenbelt area shall be reserved for turf or other appropriate surface and shall provide passive recreational space. ❑ Park benches and trash receptacles will be included at appropriate locations. ❑ HOA maintained areas may contain a fenced and gated pool and spa for the exclusive use of the Property Owners' Association members per the Association rules and regulations. ❑ Restroom facilities shall meet or exceed the requirements of applicable Building Codes. ❑ Community use buildings may be constructed for the exclusive use of the Property Owners' Association members per the Association rules and regulations. ❑ Active recreational features such as basketball half-courts may be included. ❑ Passive landscape gardens and community produce gardens are encouraged within the community. ❑ Landscape area will include clusters of shade trees dispersed throughout the recreation areas. ❑ Landscape will be designed to promote off-site visibility and public safety for policing and neighborhood awareness throughout the community.



3.7.6 OPEN SPACE

The open space elements of the plan are intended to enhance the quality of life for residents and employees of the area and provide vital environmental benefits. Open space will be privately-owned and maintained by a Property Owners Association, or as required by an applicable Development Agreement. Greenbelts, parkways, and the Mixed-Use Central Plaza shall be publicly accessible; pool areas and internal recreational amenities within the Planning Areas shall be for the private use of Specific Plan residents within the individual Planning Areas. All required irrigation shall be with City of Oxnard water and comply with the landscape guidelines of the Specific Plan.

Greenbelts

Greenbelts link the community together and provide a further separation between commercial and residential uses. Greenbelts shall be primarily turf and locally-adapted

ground cover, shrubs, and trees.

Detention Basins & Water Quality Features

Detention basins provide both flood control and water quality functions, resulting in improved outflows. Landscaping is intended to be native or adapted species and should need little or no irrigation after a grow-in period.

General Design Guidelines for Open Space

The following guidelines shall be considered when planning and designing the open space within the Teal Club Specific Plan area.



Open Space	General Design Guidelines for Open Space
<i>Greenbelts</i>	<ul style="list-style-type: none"> ❑ Shall be owned and maintained by a private Property Owners Association, or as required by an applicable Development Agreement. ❑ Design shall require approval of the City of Oxnard. ❑ Introduced vegetation shall be consistent with the landscape standards of the Specific Plan. ❑ Picnic tables and apparatus are not intended to be in the greenbelt and trail areas. ❑ Park benches and trash receptacles will be included at appropriate locations. ❑ Lighting may be allowed along the greenbelt with the consent of the City of Oxnard. Lighting shall not spill over onto adjacent residential lots.
<i>Detention Basins</i>	<ul style="list-style-type: none"> ❑ Shall be operated and maintained by a Property Owners' Association or other private entity for the purpose of flood control and water quality assurance. ❑ Design shall require approval of the City of Oxnard. ❑ Introduced vegetation shall be consistent with the landscape standards of the Specific Plan and provide suitable vegetation consistent with the engineering design requirements of the basin. ❑ No buildings shall be allowed in the detention basins. ❑ All detention basins in excess of 18" water storage depth shall be fenced and gated to restrict public access. ❑ Basins shall be maintained to the standards of the water quality permit issued by the City of Oxnard or any other agency with jurisdiction over their function. ❑ Irrigation may be temporary or permanent, subject to the final design approval of the City of Oxnard.

3.7.7 LANDSCAPE

The Teal Club Specific Plan concept seeks to unify the diverse planning elements of both the site and its surroundings, which tie the entire project together. The landscape concept also aims to provide a distinct visual impression and community identity while providing the highest level of aesthetic standards that will assure an attractive environment and enhance

the quality of life among its residents. The landscape elements will recall the Coastal California theme through the use of native and naturalized plant materials. This Coastal California sense will carry through from the drought-tolerant and indigenous plants to the hardscape features including decorative walls, signs, and monuments.



Landscape irrigation for the community will be designed to provide the most efficient means to distribute irrigation water and provide the community with the latest technology for community-wide water conservation.

A detailed planting palette has been carefully selected for the project. The primary goal is to incorporate species that are already well established, including a variety of traditional trees, shrubs, and grasses historically found

in the area. Plant selection factors include compatibility with local soils, the micro- and macro climates throughout the site, the plant's ability to merge into the existing natural environments, and drought tolerance. See Appendix 1, Landscape Palette, for the plant selections and locations. Additional plant materials may be administratively approved by the Community Development Director or their designee.

General Landscape Guidelines

In addition to City of Oxnard's Landscape Standards and the landscape provisions of this document, the following guidelines shall apply to all land uses within the Specific Plan area.

Landscape	General Landscape Guidelines for Specific Plan Area Uses
<i>General Landscape</i>	<ul style="list-style-type: none"> □ All areas not devoted to paving or building shall be landscaped and permanently maintained. □ Planting area dimensions shall be consistent with plant material requirements and the purpose of the planting, such as aesthetics, screening, environmental mitigations, air quality, wind, noise, etc. □ Street trees shall be planted at minimum of 40-foot intervals. □ Permanent automatic irrigation facilities with moisture sensors shall be provided in all landscaped areas. □ Landscape areas shall be maintained by Property Owners' or Homeowners' Associations and may be funded by Maintenance and Community Facilities District (CFD). □ Prior to the issuance of building permits for each individual segment of the plan area, landscape and irrigation plans in conformance with these regulations shall be submitted to the City of Oxnard. □ To minimize exterior water use, the following measures shall be incorporated into project design within the project area, where feasible: use of drought tolerant plants; extensive use of mulch in landscaped areas; installation of low precipitation rate irrigation systems where appropriate.

Landscape	General Landscape Guidelines for Specific Plan Area Uses
<i>General Landscape (cont.)</i>	<ul style="list-style-type: none"> □ All street trees shall be planted and staked per City of Oxnard landscape standards and details. All trees planted in turf areas shall receive turf boots to prevent damage from mowers and edgers, etc. Root barriers shall be required where trees are planted within five feet of any walls, curbs, walks, buildings, or other hardscape. □ All plant materials shall be planted in accordance with City of Oxnard standards and minimum sizing requirements. □ All landscaping and irrigation shall comply with the Water Efficient Landscape Ordinance No. 2822 and the associated Landscape Water Conservation Standards. Where conflicts may exist between the Specific Plan language and the Ordinance No. 2822 language, the more restrictive water use language shall prevail. □ Where the specific plan design guidelines are silent on landscape issues, the City of Oxnard Landscape Standards shall prevail.

Landscaping Guidelines for Residential Yards

The types of landscape, hardscape, and low yard walls or fences that are built adjacent to the streets will define the character of the neighborhood. This Specific Plan allows for front yard landscape treatments that emphasize the front lawn due to the availability of “Advanced Purified” water to the Specific Plan area. All publicly visible yards shall obtain landscape plan approval prior to construction per the requirements of Community CC&R’s.

In addition to City of Oxnard landscape standards and the landscape provisions of this document, the following shall apply to all residential yards within the Specific Plan area.



Landscape	Landscape Guidelines for Residential Yards
<i>Front Yard General</i>	<ul style="list-style-type: none"> □ Forty percent of the yard may be hardscape, providing access to the garage, guest parking, front walks, and courtyards. Hardscape may include brick, stone, interlocking concrete pavers, textured concrete, and/or impressed patterned concrete. The balance of the yard shall be landscaped. Only driveways may be used for driving and parking of automobiles. □ Landscaping may include turf, low shrubs or groundcover, and yard trees, as outlined in the Plant Palette Appendix to the Specific Plan.
<i>Front Yard Detached & Attached Cluster Home Areas (R-2)</i>	<ul style="list-style-type: none"> □ Front yards shall be a minimum of 10 feet in depth from the back of the sidewalk to a façade, including patios and porches. A setback minimum of 18 feet shall be required from the back of public sidewalk to a garage door. □ No more than 25% of the front yard may consist of turf grass.. □ At façades, low shrubs and/or groundcover shall be planted against the façade. □ At porches, low shrubs or wall vines shall be allowed. □ No pilasters, statuary, fountains or decorative columns shall be allowed in front yards.
<i>Front Yard R-3 and R-4 Areas</i>	<ul style="list-style-type: none"> □ Multi-family front yards shall be a minimum of 15 feet in depth from the back of the public sidewalk to a facade, including patios and porches. □ Turf, ground cover, and low shrubs may be planted in the area between the buildings and the sidewalk. □ Shrubs may be massed or configured as maintained hedges. □ Tree sizes and types shall be planted as buffers at the edge of the private space in proportion to the height and mass of the building façade.
<i>Other Yards</i>	<ul style="list-style-type: none"> □ Side yards shall be planted to ensure privacy and create buffers as identified above. Where practical, public street-adjacent side yards shall be planted and maintained by a Property Owners Association. □ Rear yards and property lines shall be landscaped in order to preserve the quality of the pedestrian oriented circulation system and prevent erosion.

Landscape Guidelines for Roads and Streets

The principal plantings on all streets are tree rows mixed with landscape groundcover and shrubs that define the pedestrian oriented circulation system. Perimeter and interior roads connect the Teal Club Specific Plan area to the adjacent land uses. The Plant Palette Appendix provides the Master Street Tree Plan.

In addition to City of Oxnard landscape standards and the landscape provisions of this document, the following shall apply to all roads and streets within the Specific Plan area.

Landscape	Landscape Guidelines for Roads and Streets
<i>General</i>	<ul style="list-style-type: none"> □ All streetscapes shall be planted with 24-inch box-sized trees. □ Street trees shall be selected for their ability to form canopies and require minimal maintenance; this includes trees with sidewalk-friendly root systems. □ Root collars will be utilized in all areas where there are sidewalks and/or plazas. □ Consistency in tree species and spacing shall be used to establish a strong street identity.
<i>Arterial Roads</i>	<ul style="list-style-type: none"> □ Trees on arterial roads shall be planted along the landscape buffer area adjacent to all public sidewalks with lawn or groundcover. □ Trees will be spaced equally at approximately 40 feet on center.
<i>Streets (Collector and Local)</i>	<ul style="list-style-type: none"> □ Collector and Local streets shall have trees planted along the landscape area adjacent to all public sidewalks with lawn as groundcover. □ Trees will be spaced equally at approximately 40 feet on center. □ In order to provide maximum identity, the width of landscape buffers, street medians, and sidewalks shall correspond to the type of the street.



Landscape Guidelines for Parking Lots

In addition to City of Oxnard landscape standards and the landscape provisions of this document, the following shall apply to all parking lots within the Specific Plan area.

Landscape	Landscape Guidelines for Parking Lots
<i>Parking Lots</i>	<ul style="list-style-type: none"> □ Landscaping of parking lots shall consist of 24-inch box trees and shall be provided at the rate of one tree per eight parking spaces. □ The required trees shall be planted on landscaped islands located within the parking area and along the perimeter of the parking area.

3.7.8 HARDSCAPE & STREET FURNITURE

The design of hardscape and street furniture shall create a strong community framework that relates to the established architectural style and character. Natural materials and quality construction are strongly encouraged. The hardscape and street furniture element may include enhanced paving (sidewalks and paving), walls and fences, arbors and shade

structures, water features, gates, seating elements, lighting, signage, finishes, grade elevations, and drain structures.

General Design Guidelines for Hardscape and Street Furniture

The following are guidelines for hardscape and street furniture design within the Specific Plan area.

Hardscape and Street Furniture	General Design Guidelines for Hardscape and Street Furniture
<i>General</i>	<ul style="list-style-type: none"> □ Hardscape shall be used to enhance the pedestrian experience. □ Enhanced paving shall be considered in high visibility areas, neighborhood entries, and key intersections throughout the Specific Plan area. □ Hardscape materials may include, but are not limited to, the following: integral color concrete with acid wash finish and a pattern of frequent deep score joints; seeded exposed aggregate concrete; impressed (stamped) concrete with modular tile or cobblestone pattern; pre-cast interlocking concrete pavers. □ Street furniture shall be provided at appropriate intersections and gathering places; materials and colors are to be consistent throughout the plan area.

3.7.9 CURBS & SIDEWALKS

The following are guidelines for curbs and sidewalks within the Specific Plan area.

Curbs and Sidewalks	General Design Guidelines for Curbs and Sidewalks
<i>Curbs</i>	<ul style="list-style-type: none"> □ Curbs shall be vertical concrete curb and shall be natural gray concrete.
<i>Sidewalks</i>	<ul style="list-style-type: none"> □ Sidewalks will be natural gray medium acid wash finish concrete with a tooled grid to reflect the character of older early California neighborhoods (2 ft. x 2 ft. grid for 4 ft. walks), (2.5 ft. x 2.5 ft. grid for 5 ft. walks), and (3 ft. x 3 ft. grid for 6 ft. walks). □ Other walkways in the community may include, but are not limited to, the following: natural gray acid wash concrete; integral color concrete with acid wash finish; seeded/exposed aggregate concrete; pre-cast interlocking concrete pavers.

3.7.10 LIGHTING

The following are guidelines for street and decorative lighting within the Specific Plan area.

Lighting	General Design Guidelines for Lighting
<i>Street Lights</i>	<ul style="list-style-type: none"> □ Street lights shall be decorative pole lights with traditional globe or pendant fixtures to match the architectural character of the Traditional Neighborhood community theme. □ “Cobra” lights will not be permitted within the neighborhoods. □ Poles may be concrete or aluminum; color and finish are to be reviewed by the Development Services Director. □ Lighting shall not spill over from the subject parcel to any other lot or create a hazard for pedestrians or automobile drivers.
<i>Decorative Lights</i>	<ul style="list-style-type: none"> □ Decorative up-lights, bollards, and landscape accent fixtures are allowed in the community at entries to individual neighborhoods and the commercial, mixed-use, and industrial areas. These lights shall match the architectural character of the Traditional Neighborhood community theme. □ Lighting shall not spill over from the subject parcel to any other lot or create a hazard for pedestrians or automobile drivers.



3.7.11 WALLS & FENCES

In keeping with the community theme of inclusiveness and integration, rather than exclusivity and segregation, the following are guidelines for walls and fences within the Specific Plan area.

All walls and fences shall comply with the Oxnard Municipal Code. In the event of a conflict between the Municipal Code and this Specific Plan, the more restrictive requirement shall apply.

Walls and Fencing	General Design Guidelines for Walls and Fencing
<i>Walls and Fencing</i>	<ul style="list-style-type: none"> □ The community shall be viewed as open, connective, and inviting. □ All fencing and wall heights will be measured vertically from the average finished grade at the base of the fence or wall. □ Side and rear yard walls will have a maximum height of six feet, except at the BRP buffer area where an eight foot wall is required. □ Landscaping with shrubs and vines shall be used to reduce the visible height of walls when necessary. □ Side yards requiring a retaining wall will provide a minimum horizontal separation of two feet between the side yard wall and the retaining wall to allow for planting. □ Side yard gates may be vinyl, wood or tubular steel. Perimeter fencing and walls shall be earth-tone slump stone blocks. Side yard fencing may be vinyl or earth-tone slump stone block. Finish colors must be approved by the City and be consistent with the architectural style. □ Fencing and walls must meet pool safety code requirements for height, picket spacing, and access. □ All retaining walls must be properly waterproofed and drained. □ Walls may not occur directly adjacent to any walkway; a minimum three-foot landscape area is required.

3.7.12 MAILBOXES

The following are guidelines for mailboxes within the Specific Plan area.

	General Design Guidelines for Mailboxes
<i>Mailboxes</i>	<ul style="list-style-type: none"> □ Mailboxes in the residential neighborhoods will be grouped in kiosks in accordance with the postmaster specifications. □ Mailbox kiosks shall be located at the entries to streets where possible.

3.7.13 SIGNAGE

While the primary purpose of signs is to guide and inform, the design and placement can be multifunctional. Signage for the Teal Club Specific Plan is intended to clearly identify entry to the community and reinforce the natural open space character of the plan area. The location and design of all entry signs will be directed at establishing gateways to the plan area.

numerals attached to walls, in accordance with the sign regulations of the City of Oxnard Zoning Code, Fire, and Police requirements.

Non-Residential Signage

The City of Oxnard Sign Ordinance shall apply to the Teal Club Specific Plan. The following additional requirements also apply to the Mixed Use, C-2 and BRP zones.

Residential Signage

Permanent signs for residential uses include neighborhood identification signs placed in intersection parkways and street address

Signage	General Design Guidelines for Non-Residential Signage
<i>Courtyard Wall Signs</i>	<ul style="list-style-type: none"> □ Lettering applied to courtyard walls (walls parallel to the frontage line and matching the building walls to identify facilities, institutions, and addresses) shall be individually mounted metal letters, not to exceed 16 inches in height, either front lit or halo lit. □ Courtyard walls intended to receive signage may encroach to within five feet of the frontage line.
<i>Freestanding Monument Signs</i>	<ul style="list-style-type: none"> □ Lettering applied to freestanding walls that are not courtyard walls (i.e. walls not parallel to the frontage line or matching the building walls to identify facilities, businesses, tenants, and addresses) shall be individually mounted metal letters, not to exceed 16 inches in height either front lit or halo lit except with Development Services Department approval. □ Monument signs shall meet the same setback requirements as required for courtyard wall signs and shall have distinct base and cap elements. □ Monument signs shall be set back at least five feet from the frontage line and shall not exceed five feet in height, except with Development Services Department approval. □ Materials for monument Signs may include, but are not limited to, the following: slump block with painted sack finish; plaster and stucco; brick; stone; concrete.



Signage	General Design Guidelines for Non-Residential Signage
<i>Building Wall Signs</i>	<ul style="list-style-type: none"> □ Lettering applied to a facade to identify the building or an institution within the building. See City of Oxnard Sign Ordinance.
<i>Projecting Signs</i>	<ul style="list-style-type: none"> □ Sign panels perpendicular to and projecting from a facade to identify a tenant or use shall be made of metal or wood with lettering raised, incised, or painted.
<i>Lighting, Materials, etc.</i>	<ul style="list-style-type: none"> □ All signs, if lit, shall be front lit or halo lit, not internally illuminated except with Community Development Department approval; no canister type signs are permitted. □ All signs shall be made of materials compatible with exterior building colors, materials, and finishes, and be of a high quality of fabrication. □ No signs will be permitted that do not directly relate to the name or primary service of function of the building user or institution. □ Signs are to be free of all labels and fabricator's advertising, except for those required by code. □ All electrical service to sign lights shall be fully concealed; no sign shall have exposed wiring, raceways, ballasts, conduit, or transformers. □ No sign shall have visible moving parts or simulate movement by means of fluttering, rotating, or reflecting devices. □ No sign shall have flashing, blinking, or moving lights, or any other moving lights, or other illuminating device that has changing light intensity, brightness, or color, except for parts designated to give public service information such as time, date, temperature, or similar information. □ Freestanding monument signs shall be integrated with landscaping and courtyard walls to minimize visual mass. □ Metal signs may be made of aluminum, brass, bronze, copper, or stainless steel. □ No cabinet signs allowed.

3.8 URBAN VILLAGE

The Teal Club Specific Plan is an Urban Village and fronts on a proposed transit stop at Ventura Road and Beverly Drive. It is connected to the neighborhood through a series of green belts, sidewalks and dedicated bikeways.

3.8.1 MIXED USES

An appropriate mix of uses is necessary to create a vibrant Urban Village. Neighborhood-oriented retail and commercial uses shall be located on the ground floor of Urban Village buildings fronting on the intersection of Ventura Road and Beverly Drive at the project entry. Residential uses shall be located on the floor(s) above the ground-level commercial.

3.8.2 TRANSIT ORIENTED

A new transit stop will be located on Ventura Road within the Urban Village. The transit stop will be improved with amenities including a shelter, benches and bike racks. This transit stop will connect the neighborhood to nearby job centers such as Navy Base Ventura County, the port and office and manufacturing centers to the south and east. The Oxnard Transportation Center provides greater regional links. It is intended that two or more bus routes serving the Teal Club Specific Plan will include direct stops at the Oxnard Transportation Center. This direct connection will enable public transit access to Metrolink, Amtrak, and VCTC bus service.

3.8.3 PUBLIC SPACE

The focal point for the Urban Village will be a public plaza. The plaza will front on the extension of Beverly Drive and will be integrated into the Urban Village mixed-use area. The plaza area shall have a minimum dimension of 100' in any direction and include both fixed perimeter seating and a central multi-function area for neighborhood arts and cultural use. The public plaza will include an identity feature that will further define the space. A conceptual illustration for the plaza and typical proposed features are provided in Figure 3-3.



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INFRASTRUCTURE & COMMUNITY SERVICES

4.1 INFRASTRUCTURE OVERVIEW

The Teal Club Specific Plan is proposed to be developed in two phases. Phasing is discussed in more detail in Chapter 8 and Figure 8-1 illustrates the Proposed Phase 1 Improvement Plan. Phase 1 consists of Planning Areas PA1 through PA8 as well as the perimeter and interior roadways, infrastructure and utilities necessary to serve PA1 through PA8. Additional off-site roadways and infrastructure necessary for development of Phase 1 are discussed and illustrated within the following sections of Chapter 4.

Figure 8-2 illustrates the Proposed Build-out (Phase 2) Improvement Plan. Phase 2 of the Specific Plan consists of Planning Areas PA9 through PA14 and includes the remainder of the roadway and infrastructure improvements discussed and illustrated within Chapter 4. The portions of Patterson Road and Doris Avenue that are not a

part of the Teal Club Specific Plan provide the frontage and access to the property owned by the Oxnard School District at the southeast corner of Patterson Road and Doris Avenue. Those roadway and infrastructure improvements are proposed to be built by the Oxnard School District as a part of their future campus development, as shown on their proposed campus site plans.

The extent of off-site improvements required shall ultimately be consistent with the mitigations as defined and required by the Teal Club Specific Plan certified Final Environmental Impact Report (FEIR).

4.2 CIRCULATION PLAN

The Vehicle Circulation Plan locates the existing and proposed road network and is illustrated in Figure 4-1. In addition to providing sufficient access to the proposed land uses, the street network is designed to link the project to adjacent existing communities. Final engineered road alignments may vary and are subject to review and approval by the City of Oxnard based on policies contained in this document. Any major roadway changes may require an amendment to this Specific Plan as defined in Section 8.5. The roadway categories are summarized in accordance with the Circulation Element of the City's General Plan.

4.2.1 ON-SITE STREETS

Within the Specific Plan area there are two types of street categories: Collector Streets (both Commercial/Industrial and Residential), and Local Streets.



Collector Streets

Collector Streets within the Specific Plan area will provide two 12-foot travel lanes in a 80-foot right-of-way and a curb-to-curb pavement width of 40 to 54 feet; 13-foot minimum landscaped parkway with a 5-foot sidewalk are typically provided. Figures 4-2 and 4-3, illustrate this street section.

Local Streets

Local streets are public streets located within the Specific Plan area. Local Public Streets provide access to residential and recreational land uses. They typically have a right-of-way width of 66 feet and a curb-to-curb pavement width of 40 feet with 13-foot parkway containing an 8-foot landscape area and a 5-foot sidewalk. Figure 4-4 illustrates these street sections. Local Private Streets within individual planning areas are allowed with street widths of 36 feet or less curb-to-curb, subject to City of Oxnard approval.

Local Streets are located and detailed with the following intentions:

- Provide quiet, safe, and attractive frontages for residential lots;
- Provide safe and convenient routes for pedestrians from homes to recreational areas; and
- Accommodate low volume vehicular traffic and bicycles at slow speeds.

The precise location shall be determined through the approval of subdivision maps. The precise width of private streets shall be determined through the approval of subdivision maps and related discretionary permits for these project areas.

Transit Hub

A key component of the community circulation plan is a transit hub at the southwest corner of the intersection of Ventura Road and Beverly Drive. This intersection will be traffic signal-controlled to serve as a main artery into the project and will connect the existing neighborhoods to the east to the new community park and project amenities. The transit hub will be adjacent to the Mixed-Use entry buildings and include a full bus turn-out and bus shelter for the south-bound Gold Coast Transit Ventura Road route as well as a public plaza with bike racks and seating areas to encourage local resident daily use of the public transportation system. A separate bus shelter will be provided on the east side of Ventura Road to serve the north-bound Gold Coast Transit Ventura Road route.

4.2.2 OFF-SITE STREETS

The project area is bounded by an existing network of streets that will be improved consistent with the Circulation Element of the General Plan. Two types of streets, Primary Arterial and Secondary Arterial, are adjacent to the Specific Plan area:

Primary Arterial Street

Ventura Road is currently a Four-Lane Primary Arterial Road adjacent to the project area (Figure 4-5). It consists of four 11-foot travel lanes (two in each direction), with a right-of-way width of approximately 177 feet and a curb-to-

FIGURE 4-1 VEHICLE CIRCULATION PLAN

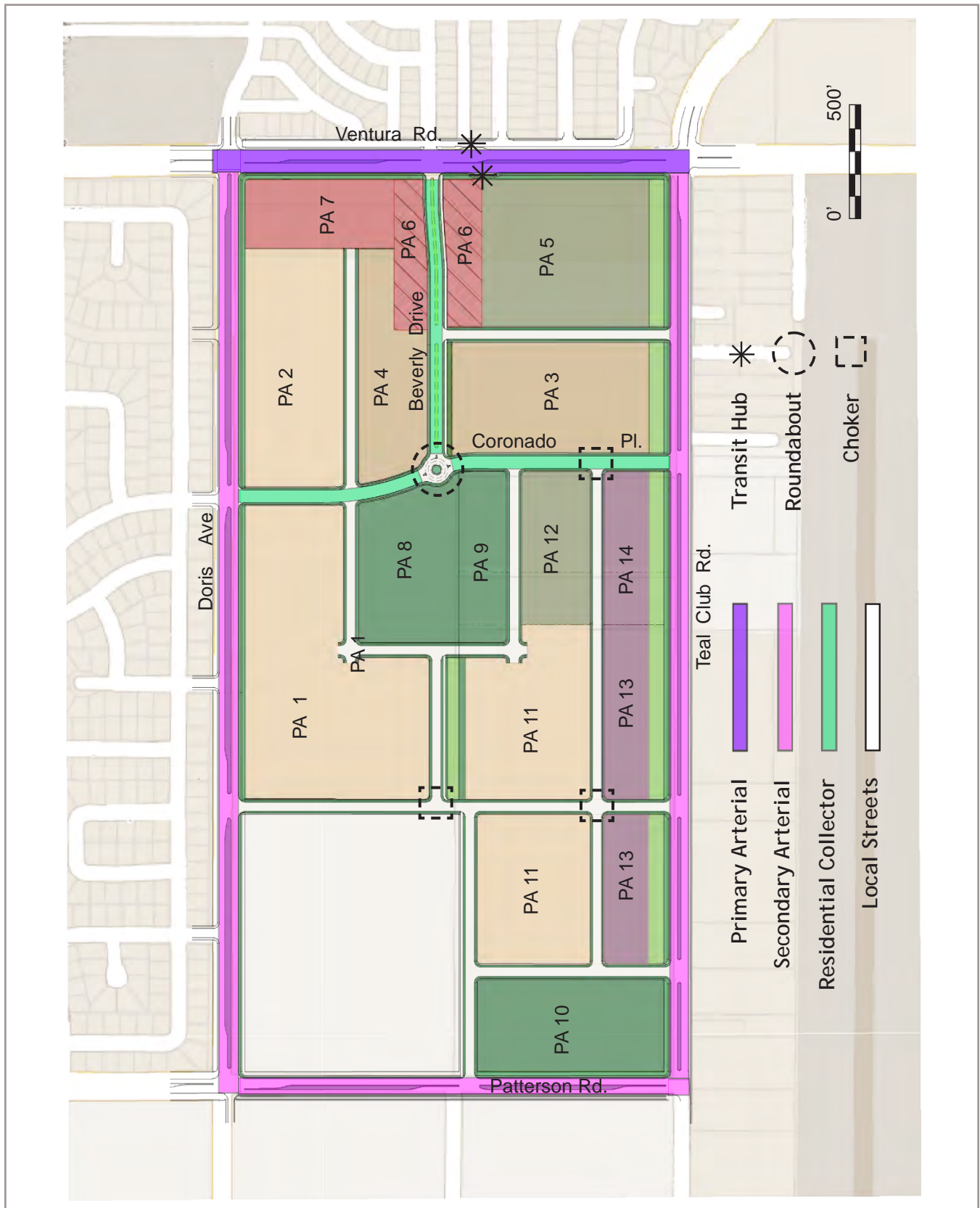




FIGURE 4-2 RESIDENTIAL COLLECTOR

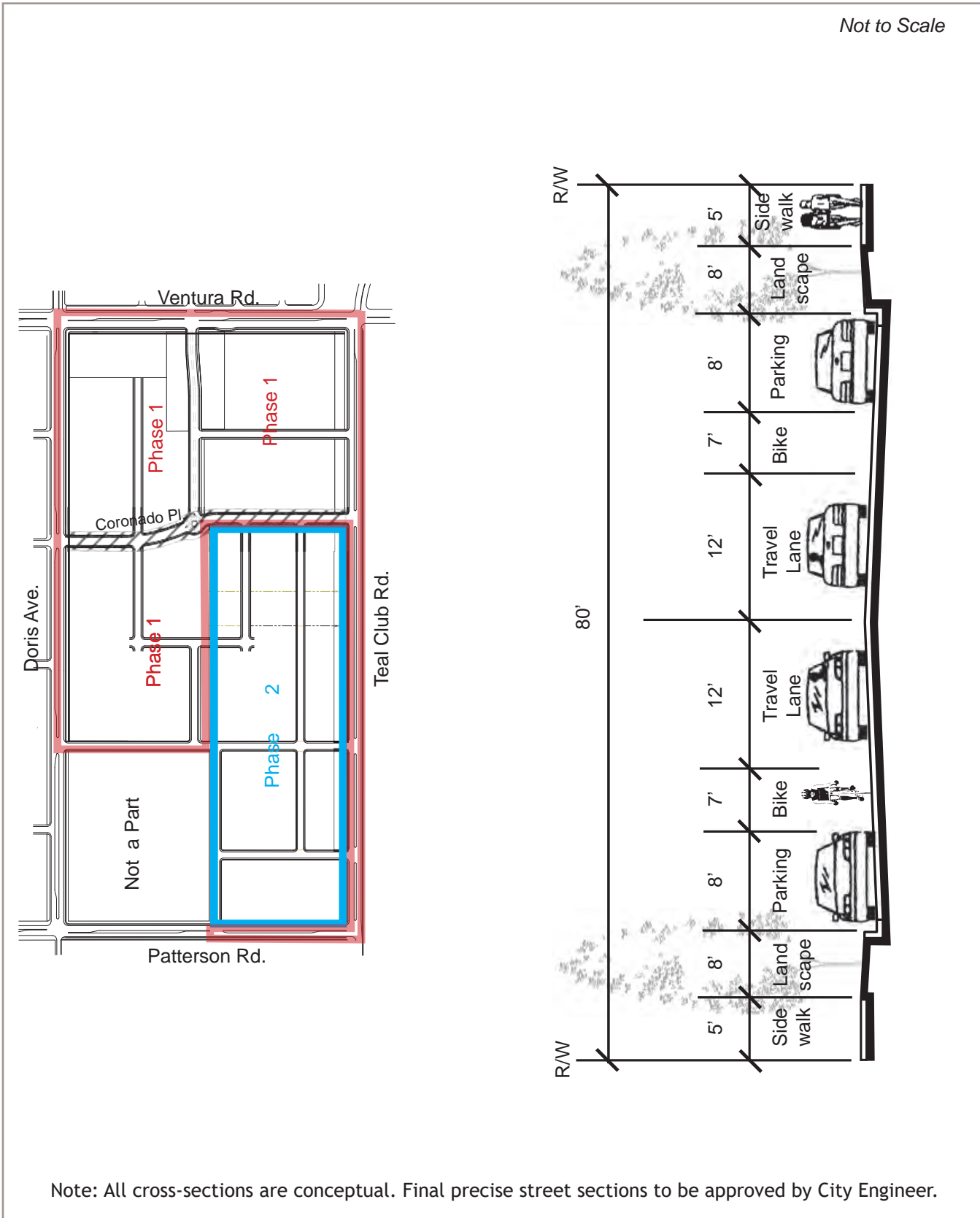
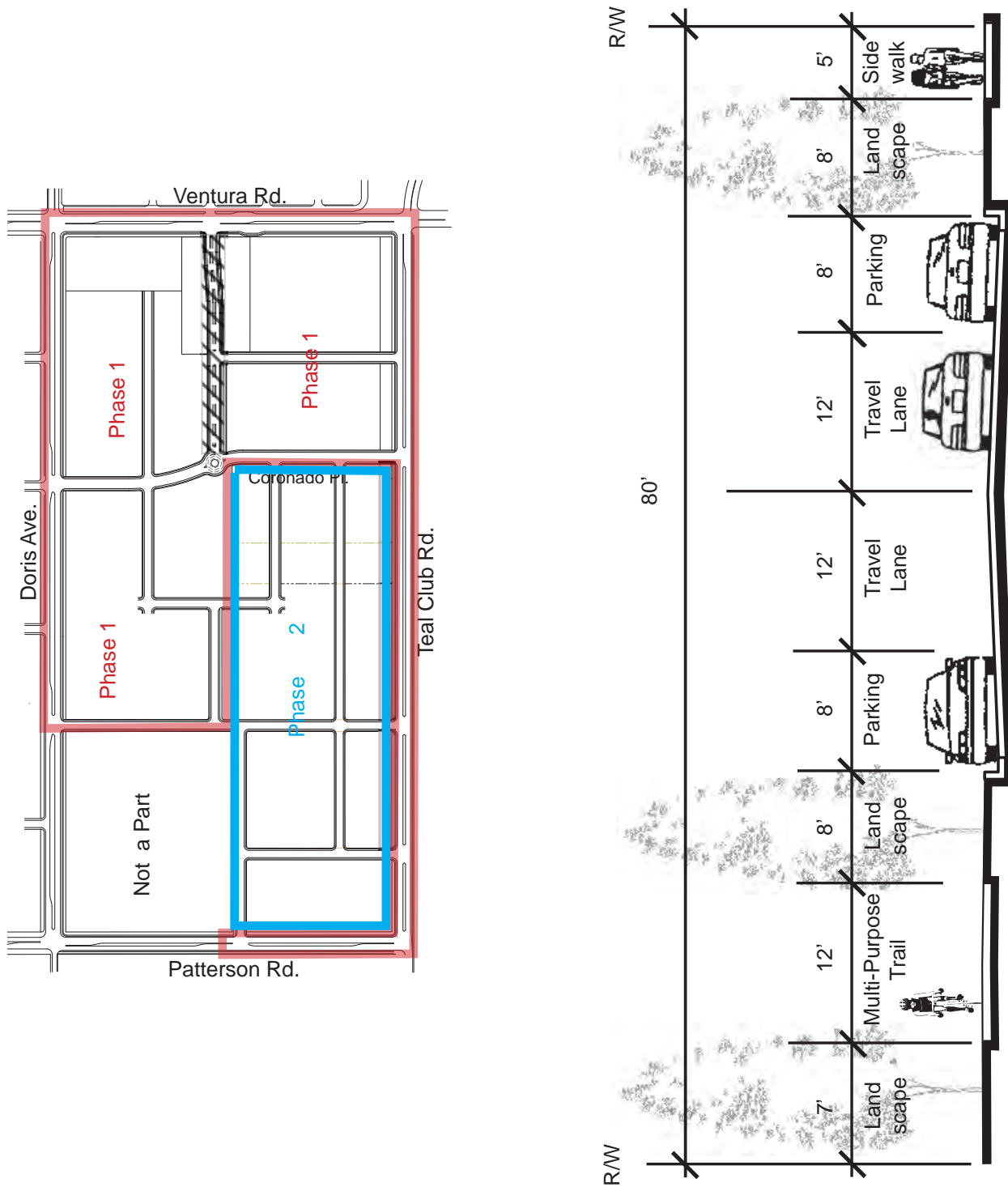


FIGURE 4-3 RESIDENTIAL COLLECTOR - BEVERLY DRIVE

Not to Scale

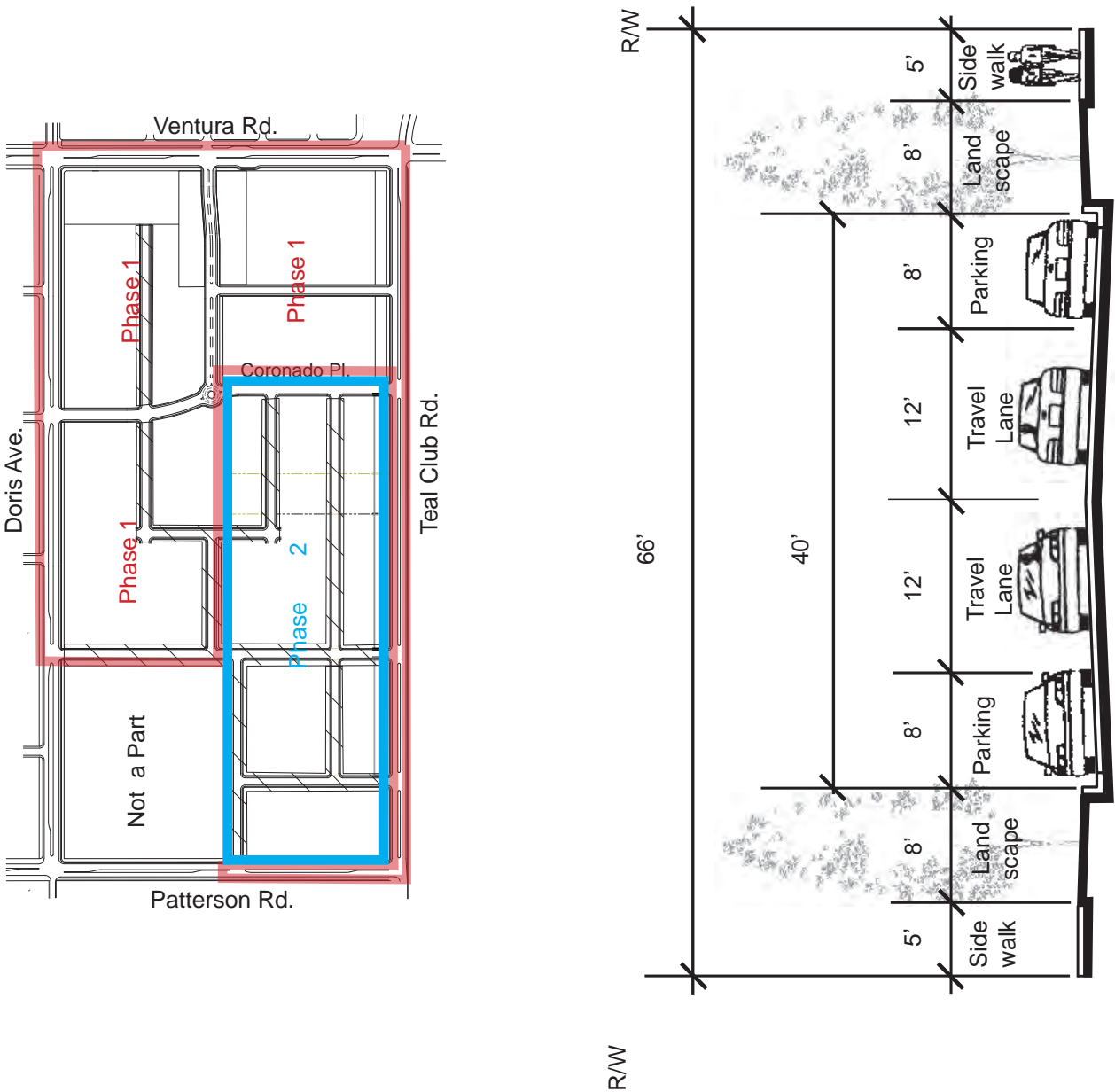


Note: All cross-sections are conceptual. Final precise street sections to be approved by City Engineer.



FIGURE 4-4 LOCAL STREETS

Not to Scale



Note: All cross-sections are conceptual. Final precise street sections to be approved by City Engineer.

curb pavement width of 102 feet including the existing frontage road, with controlled access and restricted parking. Ventura Road is proposed to be widened to six lanes plus an 8-foot Class II bike lane in each direction south of Doris Avenue, in keeping with the existing General Plan designation. A 14-foot wide landscaped center median shall be provided, and the existing parkway on the easterly side of Ventura Road shall remain as currently improved. The westerly parkway shall be 30 feet wide and consist of a 6-foot wide sidewalk and landscape areas.

Dedicated right and left turn lanes will be provided where needed. Figure 4-5 illustrates the section view of Ventura Road between Doris Avenue and Teal Club Road. The Commercial frontage along Ventura Road shall allow for one driveway curb cut between Doris Avenue and Beverly Drive to facilitate access to the Commercial site.

Prior to recordation of the first Tract Map within the plan area the Teal Club Specific Plan developer shall design the following roadway alignment details to the satisfaction of the City Engineer:

- Ventura Road & Doris Avenue intersection: design transitions to all legs of the intersection for public safety, pedestrian and bike lane connection, and free flow of traffic consistent with the General Plan Circulation Element.
- Ventura Road & Beverly Drive intersection: design transitions to all legs of the intersection for public safety, pedestrian and bike lane connection, and free flow of traffic. A traffic signal with

left-turn pockets and median shall be designed for Ventura Road at the Beverly Drive intersection. Bus turn-outs with covered bench shelters and bike storage shall be incorporated in the public right-of-way improvements.

- Ventura Road & Teal Club Road/West Second Street intersection: design transitions to all legs of the intersection for public safety, pedestrian and bike lane connection, and free flow of traffic. Roadway design shall include consideration of West Second Street median realignment and minimizing impacts to existing properties on the south side of Teal Club Road west of Ventura Road.

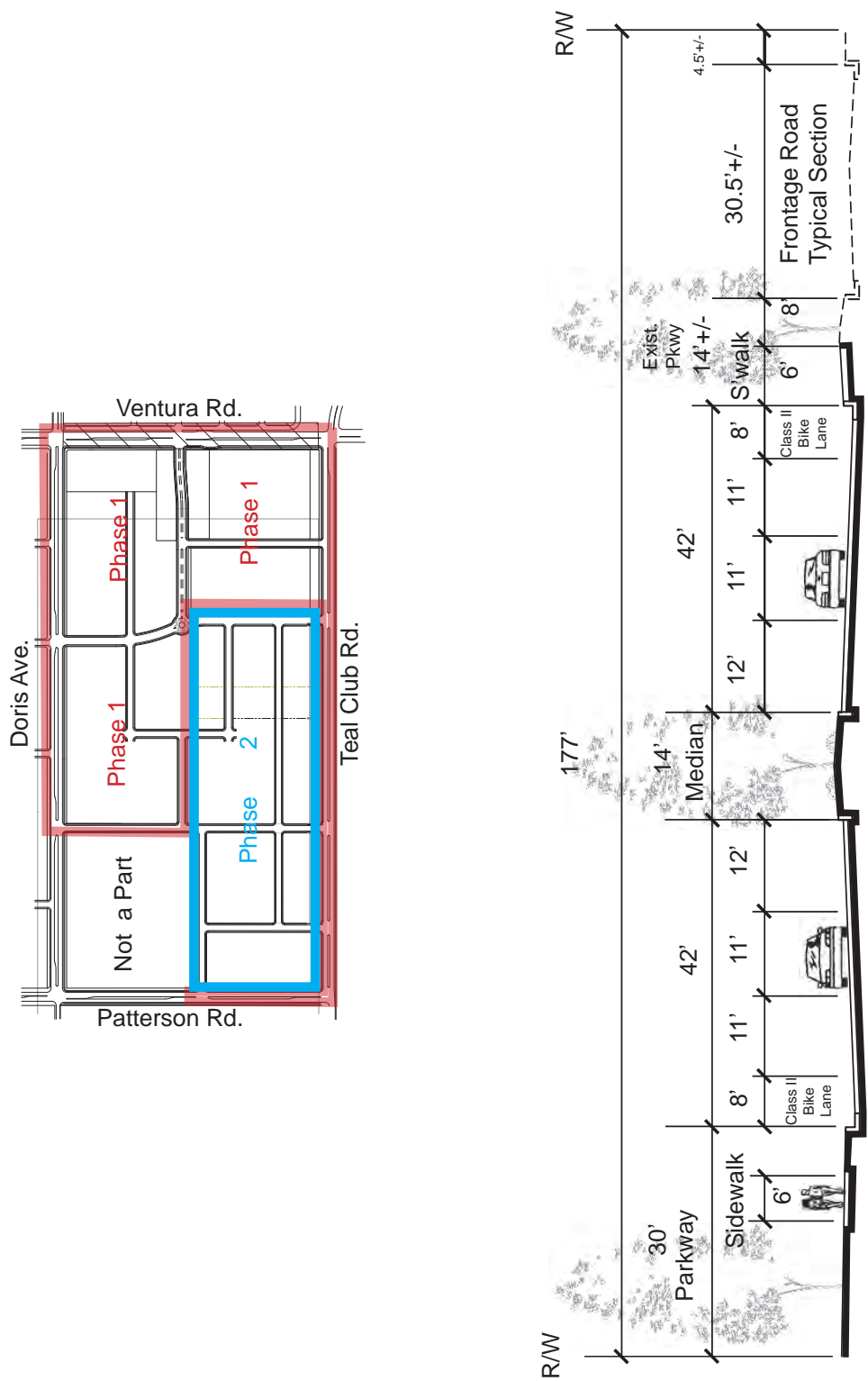
Secondary Arterial Streets

Proposed Secondary Arterial streets adjacent to the Specific Plan area include Doris Avenue, Patterson Road and Teal Club Road. In Phase 1 Doris Avenue is proposed to be widened between Ventura Road and Street "A" to two 16-foot travel lanes with a right-of way width of 98 feet, not including the existing 20' of landscape on the north side of Doris Avenue. The street section will include 8-foot Class II bike lanes and a 16-foot wide center median as shown in Figure 4-6.

Phase 1 improvements to Patterson Road shall consist of the ultimate roadway and median segments but not the east side multi-purpose trail and 30-foot parkway or the west side 10-foot parkway. Class II bike lanes shall be provided in place of the ultimate parking areas within the Phase 1 pavement section as shown in Figure 4-7.

FIGURE 4-5 PRIMARY ARTERIAL - VENTURA ROAD

Not to Scale



Note: All cross-sections are conceptual. Final precise street sections to be approved by City Engineer.

As a part of Phase 2 of the Specific Plan Patterson Road along the Phase 2 frontage shall provide two 16-foot travel lanes with a right-of-way width of 104 feet. The street section will include an 8-foot on-street parking area on the east side only, an 8-foot bike lane on the west side, a 16-foot wide center median, and a 12-foot wide multi-purpose trail within a 30-foot wide parkway on the east side as shown in Figure 4-8.

Phase 1 improvements to Patterson Road shall consist of the ultimate roadway and median segments but not the east side multi-purpose trail and 30-foot parkway or the west side 10-foot parkway. Class II bike lanes shall be provided in place of the ultimate parking areas within the Phase 1 pavement section as shown in Figure 4-7.

Teal Club Road between Ventura Road and Patterson Road is to be widened in two phases. Phase 1 widening is shown in Figure 4-9 and includes full build-out from Ventura Road to the new Phase 1 extension of Coronado Place through the Specific Plan area. Full build-out provides for four 12-foot travel lanes with a right-of-way width of 122' and a curb-to-curb pavement width of 78 feet. The pavement section will provide for 8-foot Class II bike lanes and a 14-foot wide center paved and striped median as shown in Figure 4-10. The paved and striped median will allow for the continued left-turn access in and out of existing multiple driveways on the south side of the road.

Phase 1 widening of Teal Club Road west of Coronado Place to Patterson Road will consist of improvements within the existing 40-foot right-of-way with two travel lanes and 8-foot paved shoulder areas on each side as shown in Figure 4-9. Phase 1 widening from Patterson Road west to Victoria Avenue will also consist of the two travel lanes and 8-foot paved shoulder areas as shown in Figure 4-9.

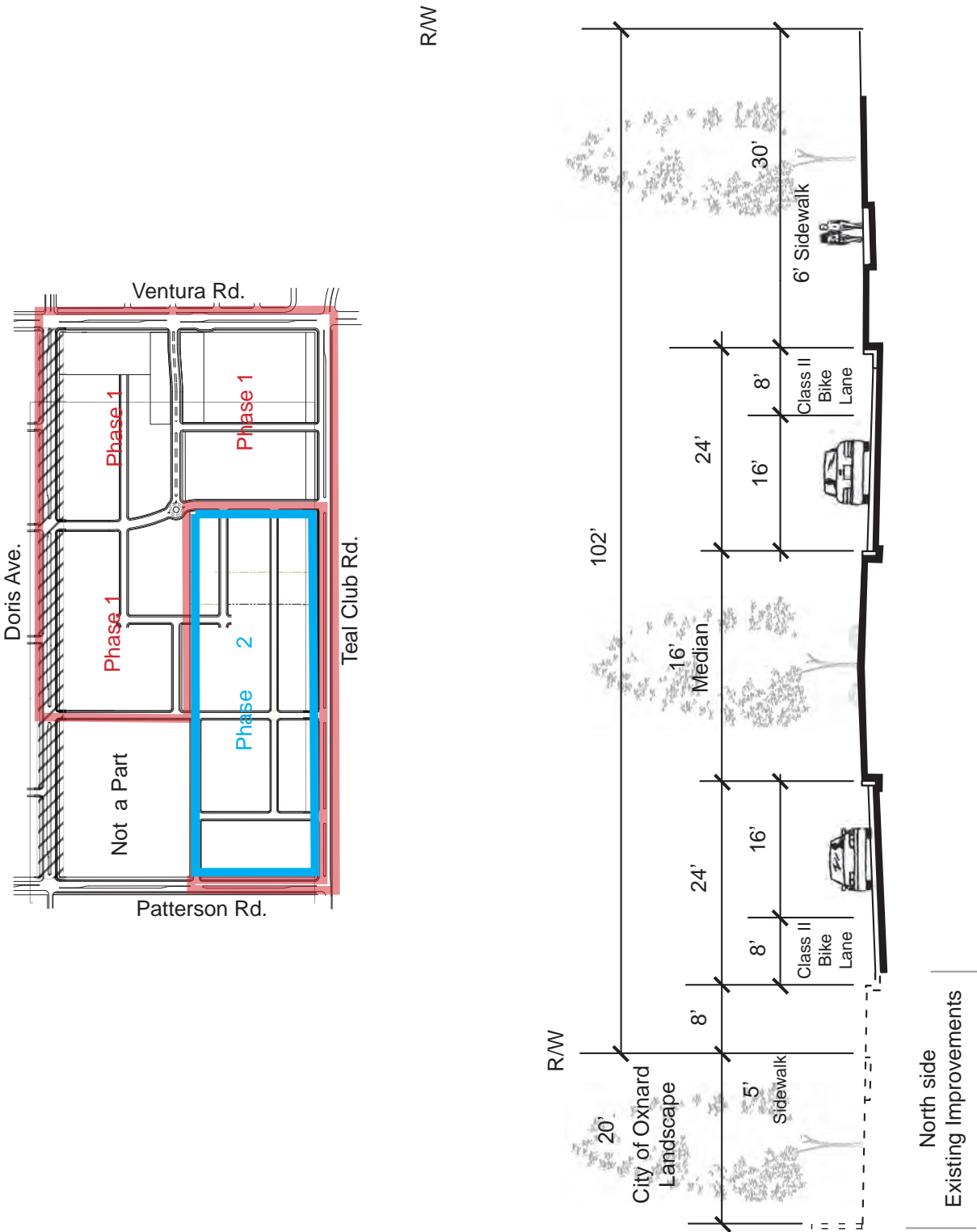
Phase 2 widening of Teal Club Road from Coronado Place to Patterson Road will consist of the full build-out of Teal Club Road as shown in Figure 4-10 with four 12-foot travel lanes, 8-foot Class II bike lanes and a 14-foot wide center paved and striped median. No further improvements are proposed to Teal Club Road west of Patterson Road except as may be require for traffic movements at the Teal Club Road intersections with Patterson Road and Victoria Avenue.

Dedicated left turn lanes will be provided on secondary arterial roads where needed, subject to City of Oxnard approval.



FIGURE 4-6 SECONDARY ARTERIAL - DORIS AVENUE

Not to Scale



Note: All cross-sections are conceptual. Final precise street sections to be approved by City Engineer.

FIGURE 4-7 SECONDARY ARTERIAL - PATTERSON ROAD PHASE 1

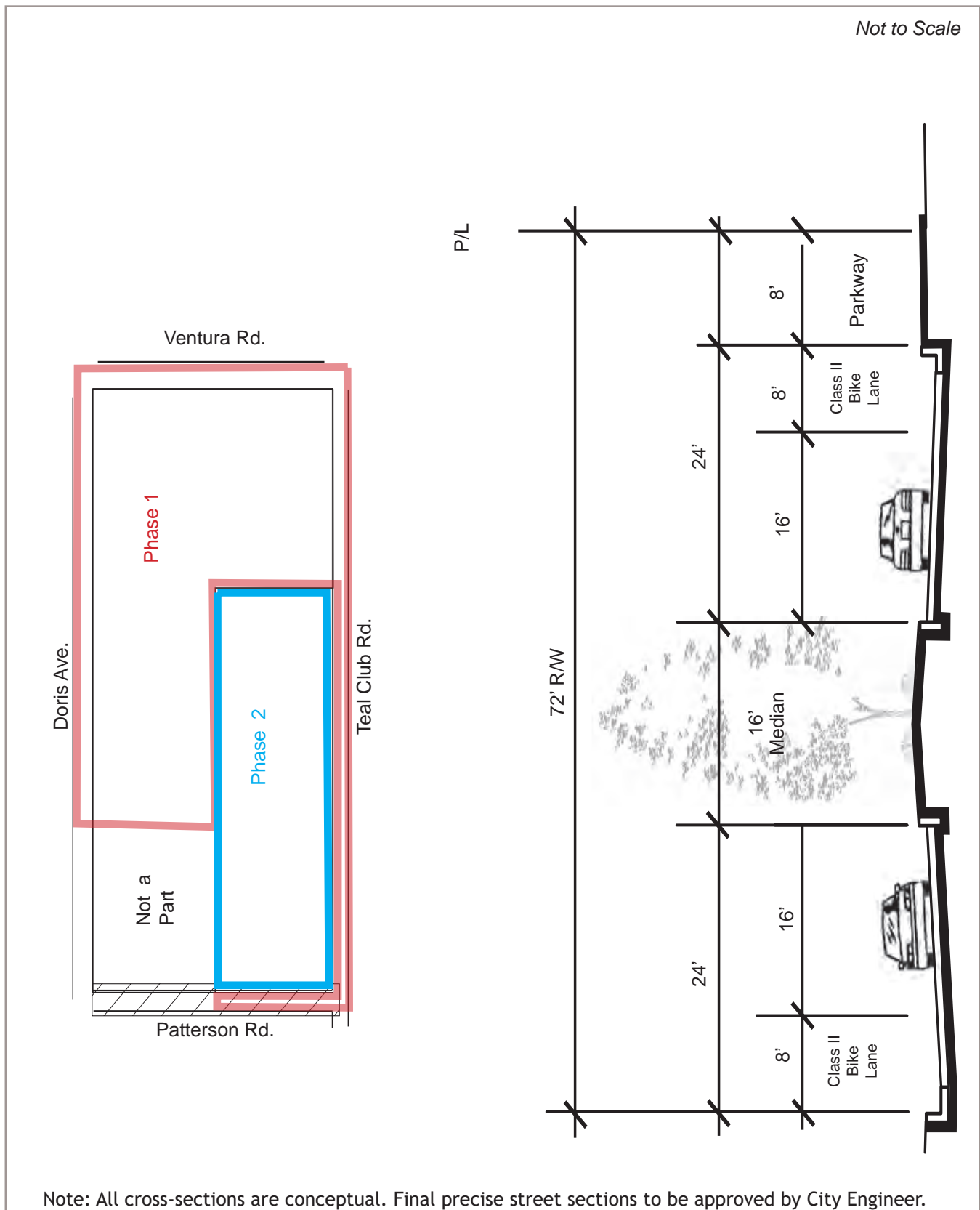
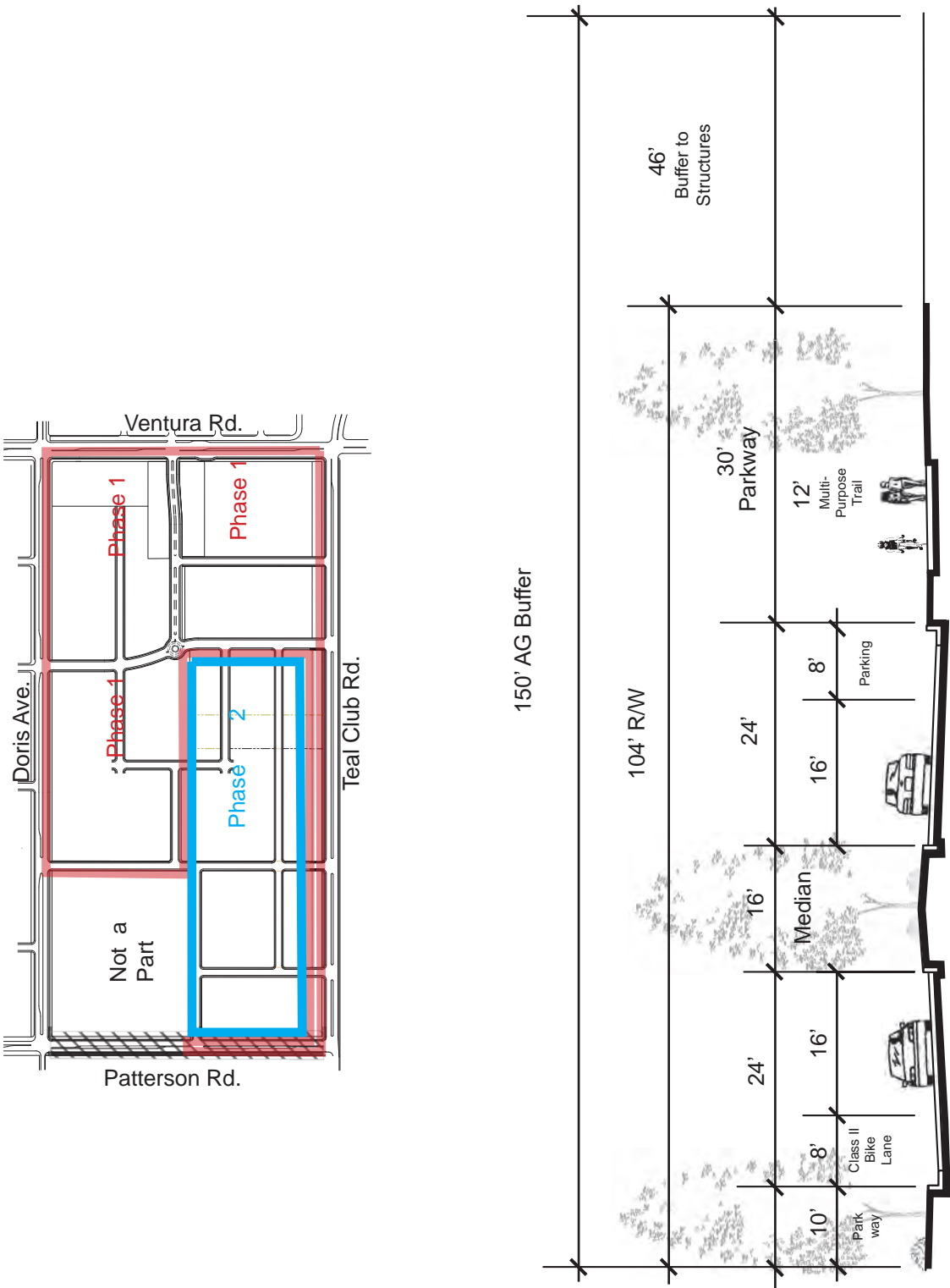


FIGURE 4-8 SECONDARY ARTERIAL - PATTERSON ROAD (ULTIMATE)

Not to Scale



Note: All cross-sections are conceptual. Final precise street sections to be approved by City Engineer.

FIGURE 4-9 SECONDARY ARTERIAL TEAL CLUB ROAD - PHASE 1

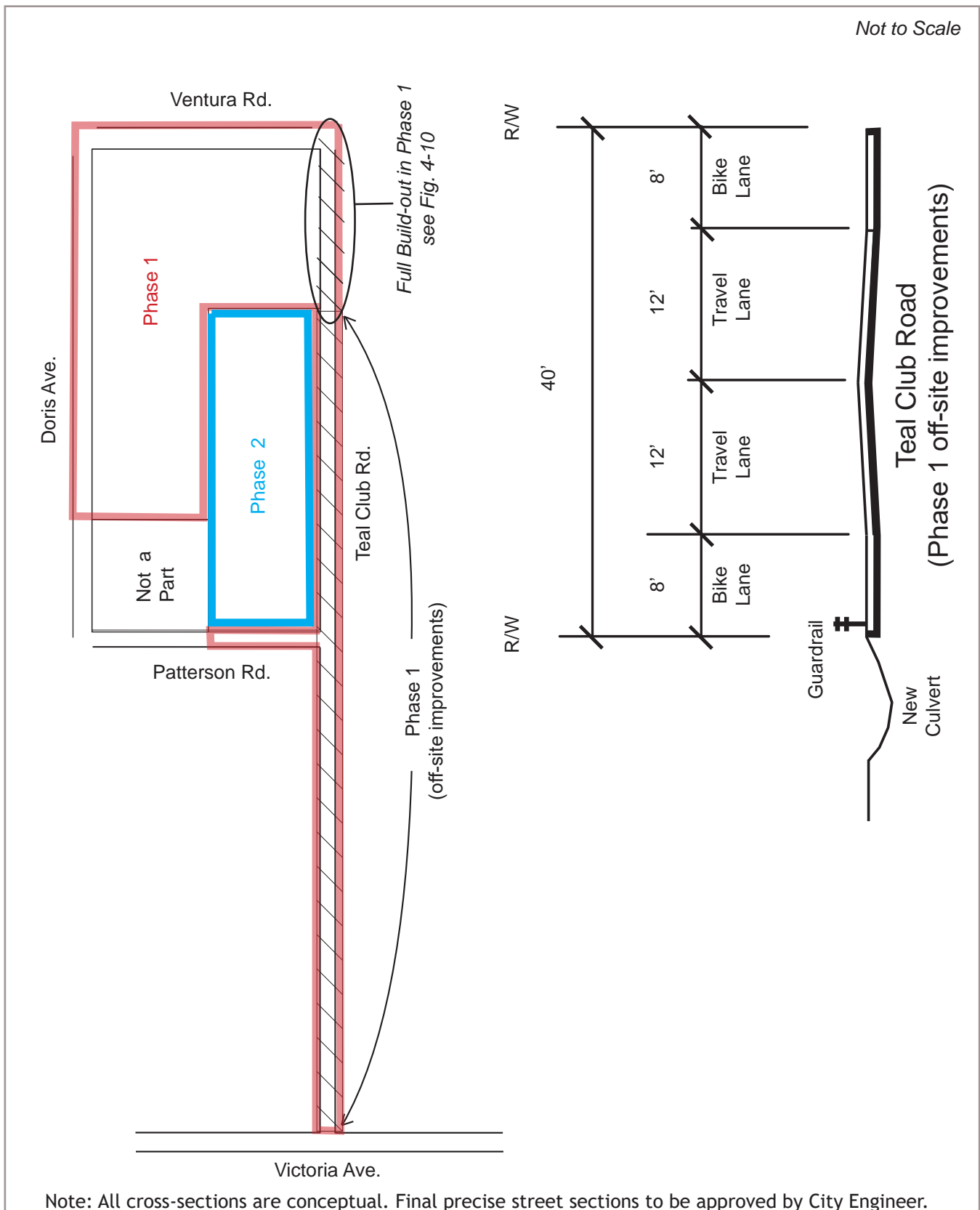
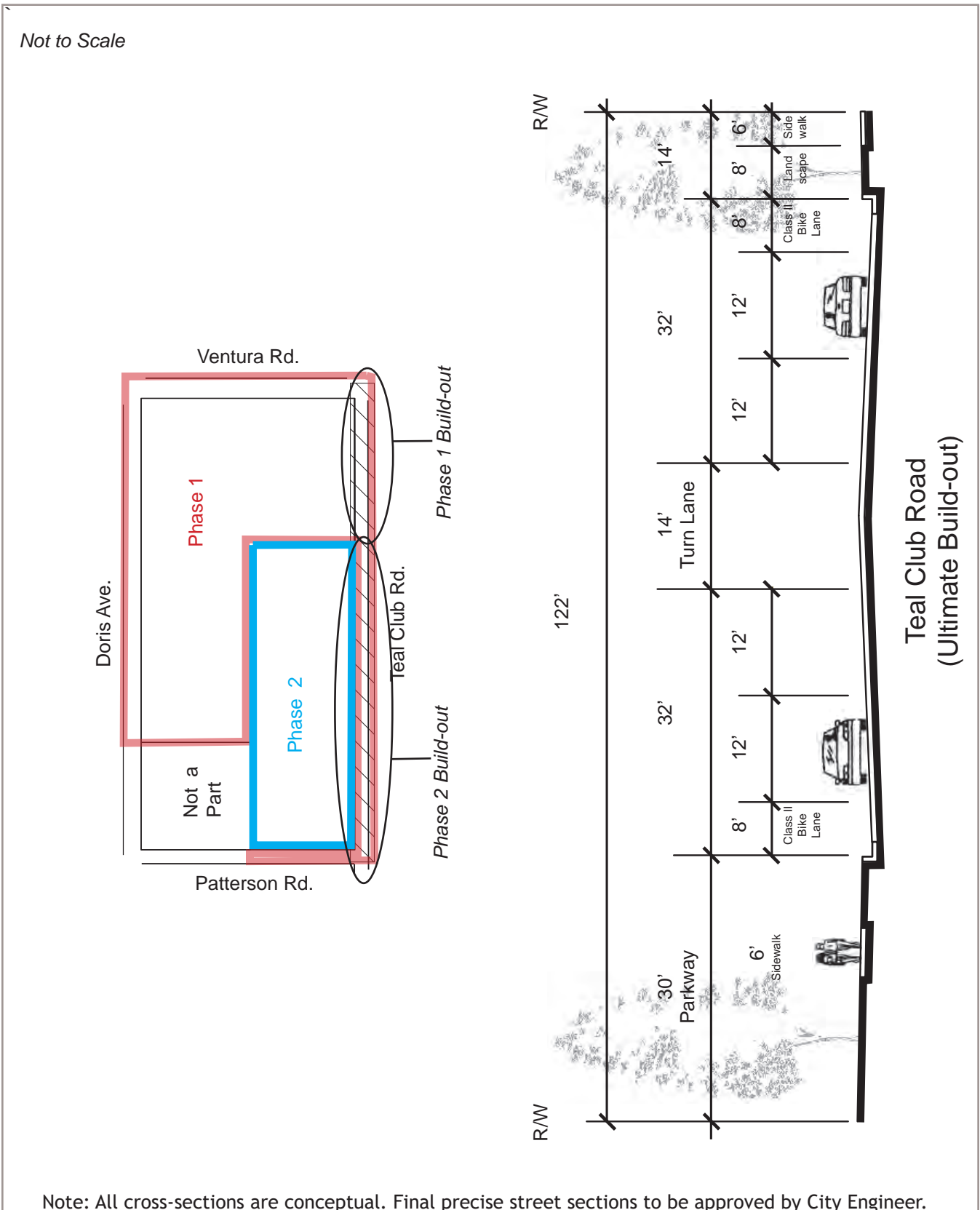




FIGURE 4-10 SECONDARY ARTERIAL - TEAL CLUB ROAD (ULTIMATE)



4.2.3 BIKE LANES & TRAIL NETWORKS

The bike lane system for the Specific Plan area is intended to promote walking and biking within the community, thereby reducing automobile trips and related environmental impacts. Figure 4-11 illustrates the bike lane plan.

The bike lane system within the Specific Plan area provides safe connections between residences, employment centers, Public/Semi-Public uses, the community park, and transit hub. Arterial roads and Collector roads shall include clearly marked Class II bike lanes.



FIGURE 4-11 BIKE PATH CIRCULATION



4.3 PUBLIC FACILITIES

This section addresses the public service requirements for the Teal Club Specific Plan area. All necessary utilities of sufficient capacity are either adjacent to or will be brought to the site as part of the Teal Club project. Figure 4-12 illustrates existing public schools, fire stations and civic buildings near the project area. Utility sizing in the Specific Plan is preliminary and project-specific studies will be required at the time of subdivision approval. Maintenance funding for Specific Plan infrastructure shall be provided by Maintenance Community Facilities District (CFD) or Community Service Area, subject to approval by the City of Oxnard.

4.3.1 POTABLE WATER

A goal of the project is to minimize potable water use. To provide adequate potable water for the project, the existing agricultural water rights within the Teal Club Specific Plan area will be transferred for Municipal and Industrial uses to the City of Oxnard, subject to the approval of the Fox Canyon Groundwater Management Agency (FCGMA). The agricultural water rights for each property will be transferred to the city after the approval of a final tract map and prior to the start of construction. There is no obligation to transfer agricultural water rights unless a development project consistent with the Specific Plan is fully approved including a final tract map that allows for the construction of the proposed residential units. The City of Oxnard will provide potable water to the site.

The City of Oxnard will be the source of domestic water for the project. Transmission mains exist within the plan area and the Developer will construct any upgrades required to provide potable water service

to the entire Specific Plan area. The precise configuration of water service system for the proposed project will be determined at the time individual tract maps are prepared for each phase of the Specific Plan. Figures 4-13 and 4-14 illustrates the proposed Potable Water System.

4.3.2 ADVANCED WATER PURIFICATION WATER SYSTEM

The City of Oxnard will provide water service to the project area and encourage water efficient landscapes and conservation. Greenbelts, common areas, and similar landscapes shall all utilize “Advanced Water Purification” water for landscape irrigation purposes.

4.3.3 WASTEWATER SYSTEM

The City of Oxnard will provide sanitary sewer service for the project site. All existing and proposed wastewater lines drain through a network of pipes to the Oxnard Wastewater Treatment Plant on Perkins Road. To reduce potential impacts to the off-site lines, the east side of the project area will connect to the Redwood trunk in Ventura Road and the west side of the project will flow to the trunk in Victoria Avenue. The exact configuration of the sewage collection system for the project will be determined at the time individual tract maps are prepared for each phase of the Specific Plan. Figures 4-15 and 4-16 illustrates the proposed Phase 1 and Build-out (Phase1 and Phase 2) Wastewater System.

The City of Oxnard requires that new development connect to the city’s existing wastewater system. Any developer constructing a new wastewater line is required to coordinate the construction and dedication of any such wastewater system with the city for future operation and



maintenance. The developer will pay for its proportionate share of the off-site wastewater mitigations identified in the FEIR. Wastewater lines within private streets will be operated and maintained by a Community Facilities District or Service Area.

4.3.4 STORMWATER DRAINAGE

The Teal Club Specific Plan area is located in the Oxnard Plain Hydrologic Area of the Santa Clara – Calleguas Hydrologic Unit. Storm water from the on-site agricultural fields currently drains to the west and south from the project site to a manmade channel along the southern boundaries of the project area.

The design and sizing of the Teal Club Specific Plan drainage system is based on a 100-year storm event. The exact configuration of the drainage system for the project will be determined at the time individual tract maps are prepared for each phase of the Specific Plan. The proposed Phase 1 and Build-out (Phase 1 and Phase 2) Stormwater Drainage System drainage system is illustrated in Figures 4-17 and 4-18.

The design and sizing of the Teal Club Specific Plan drainage system and water quality treatment system shall comply with the applicable National Pollution Discharge Elimination System (NPDES) MS4 Permit in force at the time of development, subject to the interpretation of the governing body regulating permit issuance. All facilities within the Specific Plan area shall be funded,

permitted, and maintained in satisfactory order by a Master Property Association, Community Facilities District, or other private entity as approved by the City of Oxnard. Stormwater lines within private streets that primarily serve private streets will be operated and maintained by the Home Owners' Association.

4.3.5 SOLID WASTE DISPOSAL

The City of Oxnard-owned Del Norte Regional Recycling and Transfer Station (DNRRTS) will provide solid waste disposal services to the project area. Any remaining refuse from the project that cannot be accommodated by the DNRRTS will be hauled to other landfill sites in Ventura County. User fees for collection and disposal services will be incurred for development within the project area.

FIGURE 4-12 PUBLIC FACILITIES

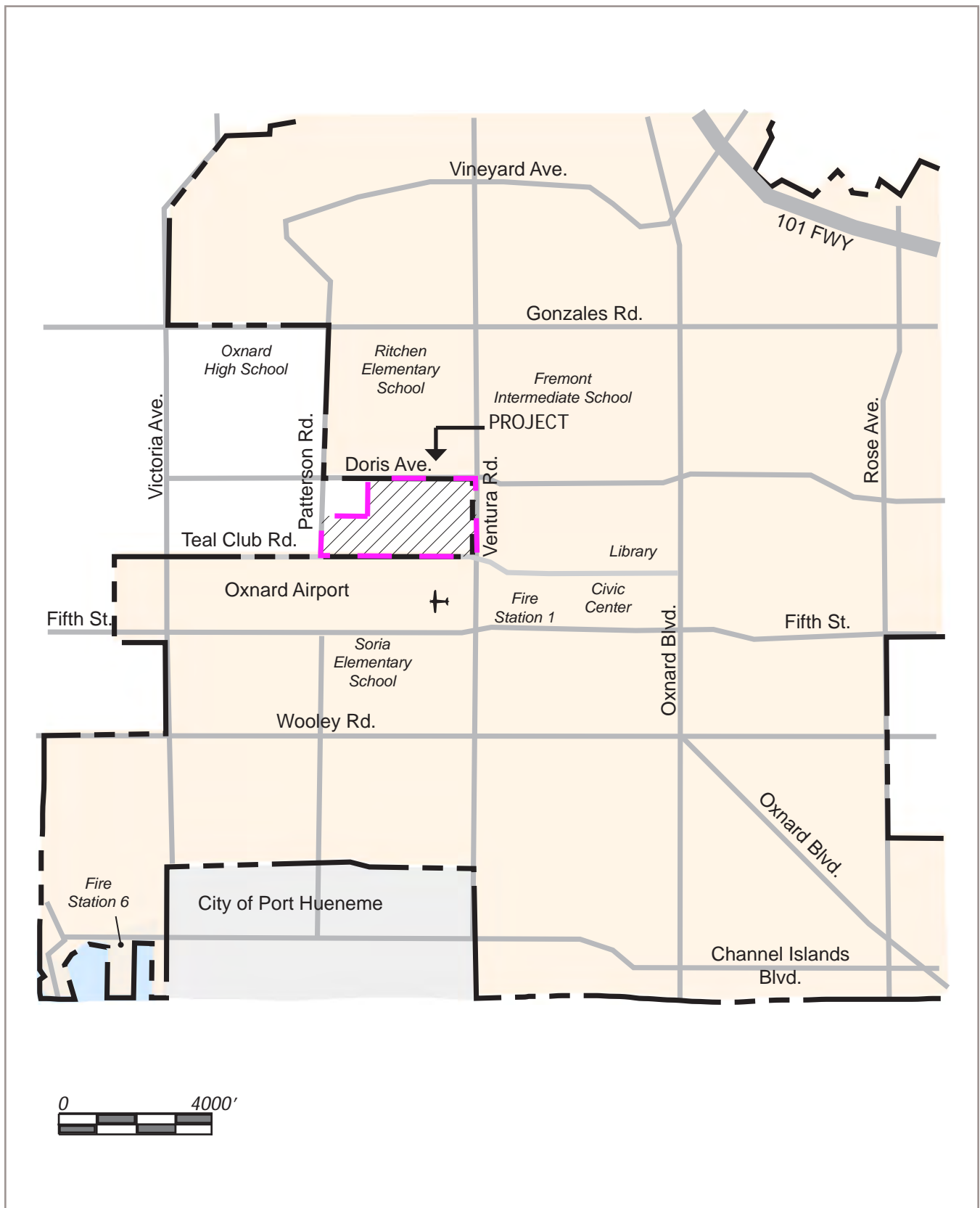




FIGURE 4-13 PROPOSED PHASE 1 WATER SYSTEM

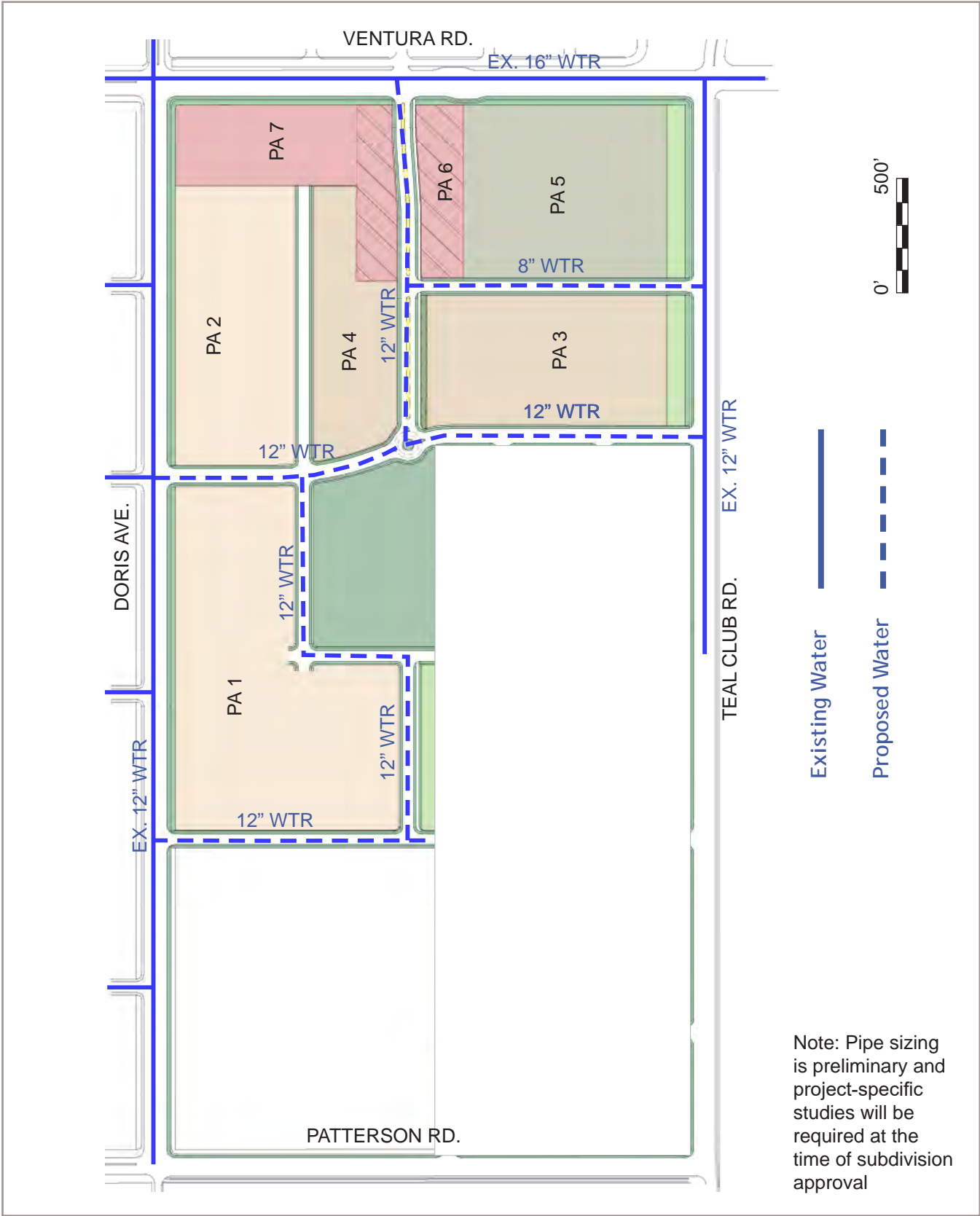


FIGURE 4-14 PROPOSED BUILD-OUT WATER SYSTEM

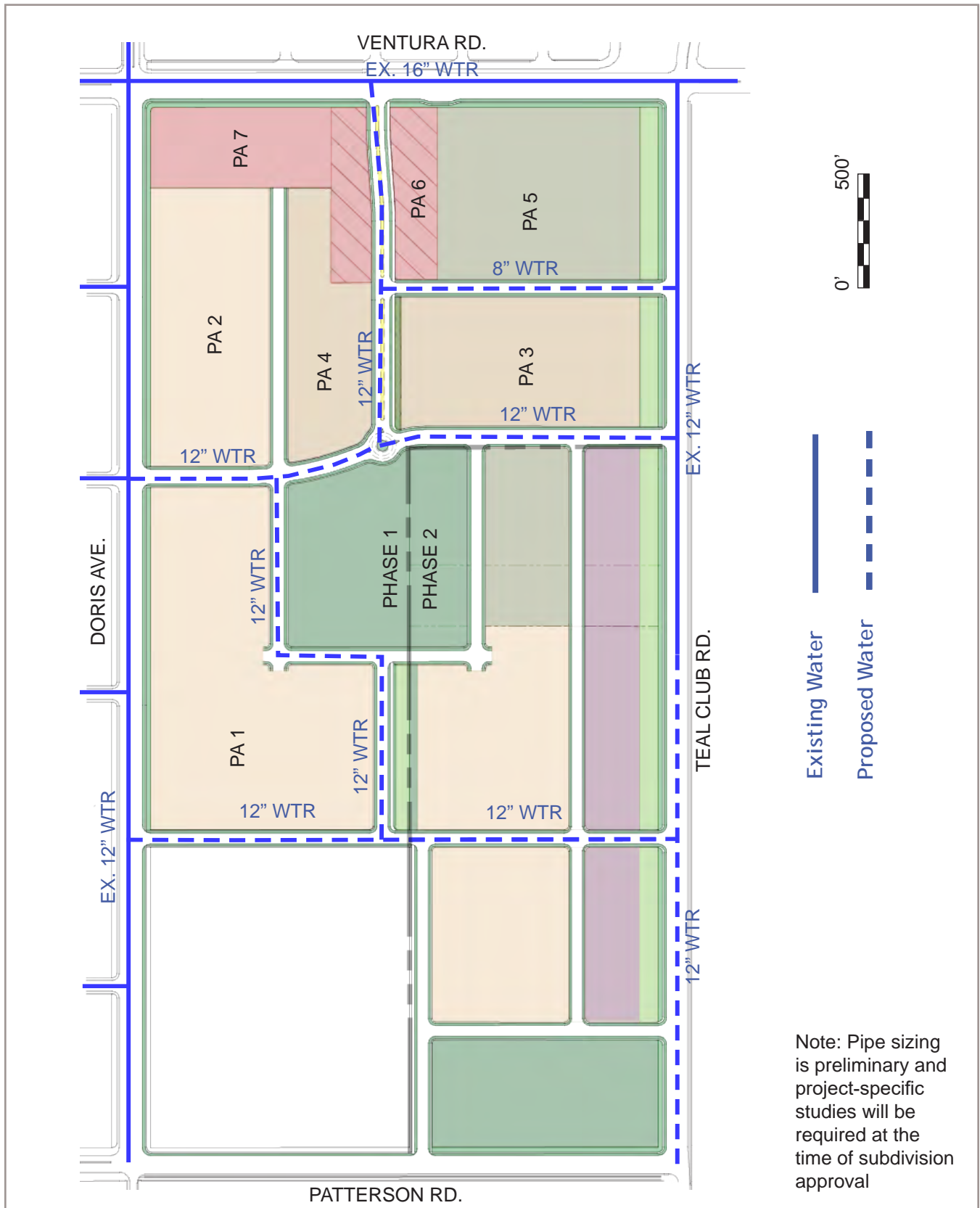




FIGURE 4-15 PROPOSED PHASE 1 WASTEWATER SYSTEM

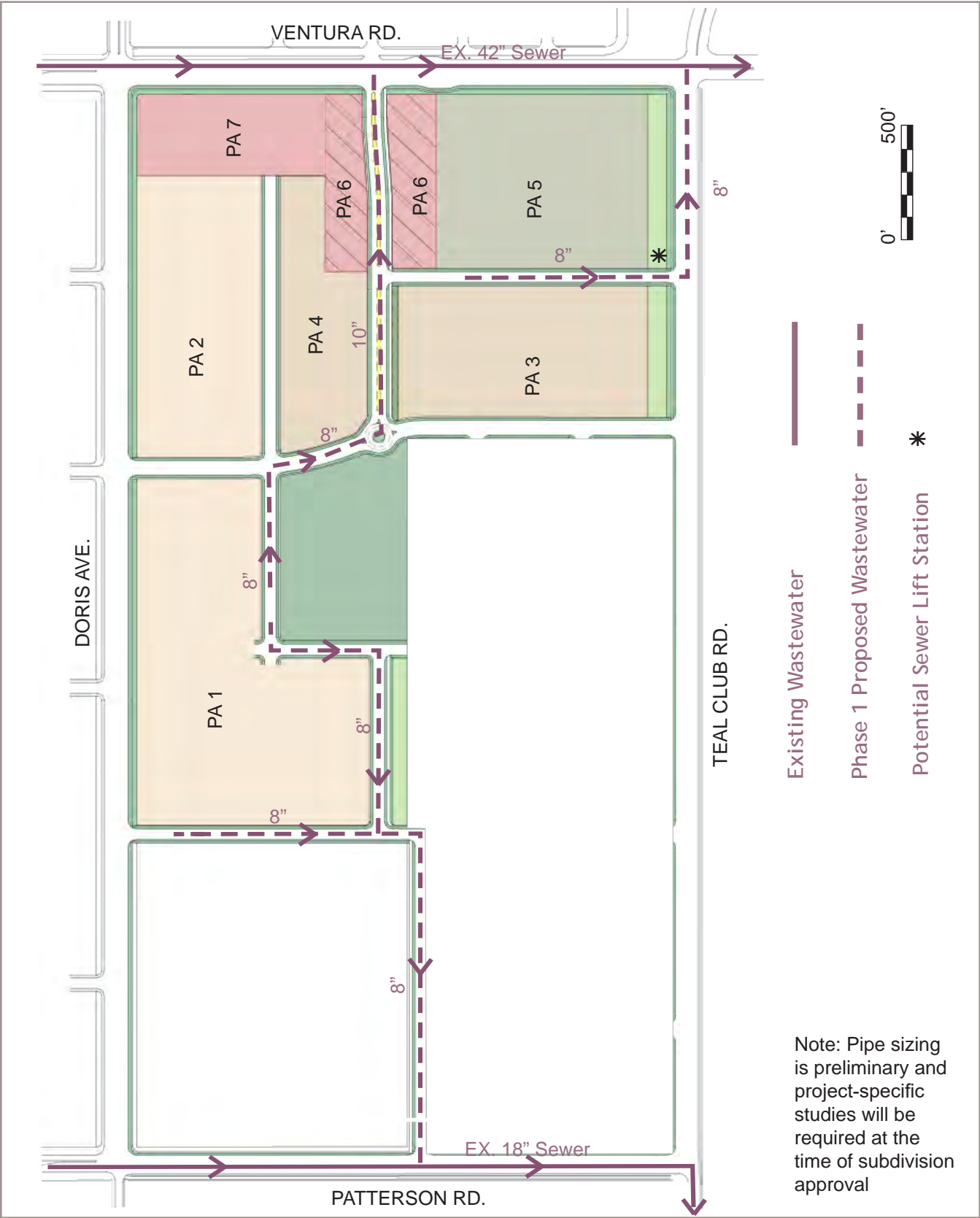


FIGURE 4-16 PROPOSED BUILD-OUT WASTEWATER SYSTEM





FIGURE 4-17 PROPOSED PHASE 1 STORMWATER DRAINAGE SYSTEM

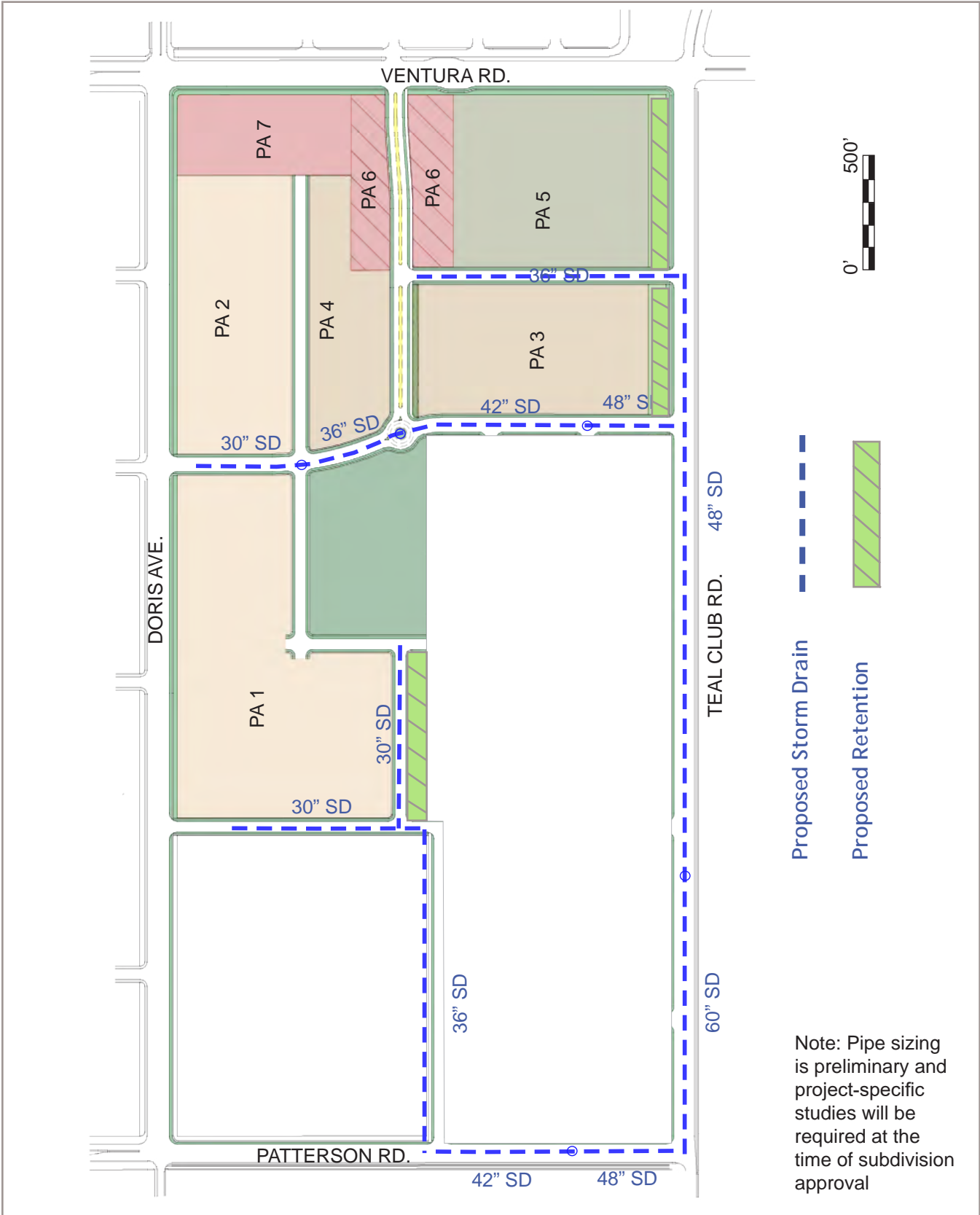
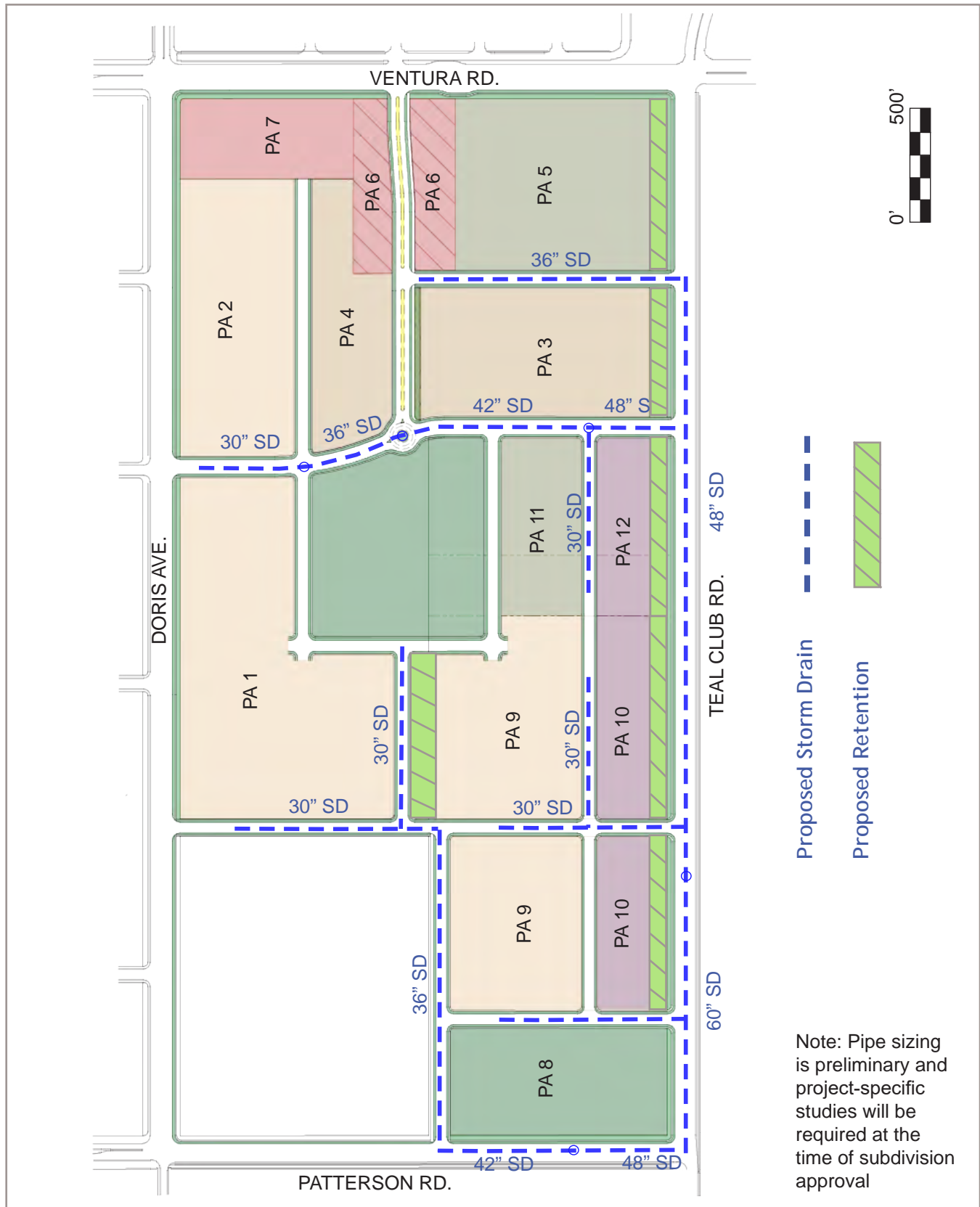


FIGURE 4-18 PROPOSED BUILD-OUT STORMWATER DRAINAGE SYSTEM





4.3.6 NATURAL GAS

The Southern California Gas Company (SCG) supplies gas resources to the City of Oxnard. The existing SCG facilities will serve as primary sources of gas supply for the Teal Club Specific Plan. SCG will review the proposed gas piping layout at the design development phase.

4.3.7 ELECTRICITY

Southern California Edison (SCE) provides electricity to the project area. The existing SCE facilities will serve as the primary sources of electrical supply for the Teal Club project. All future electrical additions located within the project boundaries will be placed underground.

4.3.8 PUBLIC SCHOOLS

Oxnard School District (OSD) and Oxnard Union High School District (OUHSD) provide public education services to the project area. Development of approximately 990 residential units proposed as part of the Specific Plan would introduce additional students into the OSD and OUHSD and increase demand on school facilities. School fees are used to mitigate impacts associated with adding incremental amounts of students to school districts. OSD has acquired a 25-acre site immediately adjacent to the Teal Club Specific Plan at the southeast corner of Doris and Patterson Roads. The district proposes to construct school facilities to serve the Kindergarten through 8th Grade students within the surrounding community, including the Teal Club Specific Plan area. OUHSD will house students generated by the project in existing or future facilities.

4.3.9 POLICE PROTECTION

The Teal Club Specific Plan is served by the City of Oxnard and the Ventura County

Sheriff's Department since the property is within the city's Sphere of Influence but not within its current corporate limits. Once annexed to the city, the Specific Plan area will be served by the City of Oxnard Police Department.

Developers within the Teal Club Specific Plan shall coordinate with the city's police department regarding the design standards and safety protection requirements and will implement these in the final design and construction of all residential, commercial and public structures.

4.3.10 FIRE PROTECTION

The Teal Specific Plan area is served by the Oxnard Fire Department. Two fire stations are located in proximity of the project area; Fire Station 1 and Fire Station 6. Fire Station 1 is located at 491 South "K" Street, approximately 1.2 miles east of the Specific Plan area and would be the primary response company. Fire Station 6 is located at 2601 Peninsula Road, approximately 2.3 miles northeast of the project site.

All development within the Specific Plan shall pay applicable City of Oxnard Fire Protection fees in place as required by City Ordinances. The use of non-flammable materials, especially roofing materials, will be required for all structures in the Specific Plan area.

4.3.11 PARKS & RECREATION

There are two public parks in the plan area: the Phase 1 and 2 portions of the ten-acre centrally located Neighborhood Park and the 7.3 acre Phase 2 southwest area park. The central east-west greenbelt creates a visible connection for public gathering, organized recreation, walking, and biking.

Both public parks will be owned and maintained by the City of Oxnard and serve a portion of the recreational needs of the plan area and the existing nearby neighborhoods. Maintenance funding shall be provided by a city-approved funding mechanism established and formed by the developers of the Specific Plan and financed by all development within the Specific Plan as provided for in the Project Development Agreement(s). Turf play areas, playground equipment, bathrooms, and picnic tables will be incorporated into these parks, subject to final design approval by the City of Oxnard.

Private recreation facilities shall be included within each multi-family project to meet a portion of recreational needs as determined during the Planned Development Permit approval process.



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5

ENVIRONMENTAL RESOURCES

5.1 OVERVIEW

The City of Oxnard strives to be a leader in environmental resource sustainability. This chapter focuses on implementing long-established state and federal regulations and should be used in conjunction with Chapter 2 - Sustainable Community, which focuses on new environmental issues such as climate change.

5.2 WATER RESOURCES

The Teal Club Specific Plan provides important implementation measures for the city's water resource management goals.

5.2.1 WATER SUPPLY

The Teal Club Specific Plan provides for the transfer of historic agricultural pumping rights to the city for municipal uses. The transfer reduces the amount of water that may be extracted from the aquifer and the existing wells will be capped.

5.2.2 ADVANCED WATER PURIFICATION WATER

The Teal Club site is located on Ventura Road where the City of Oxnard water infrastructure has been installed. With the addition of the Advanced Water Purification program the water distributed throughout the project site will be primarily locally sourced and will reduce the environmental impacts associated with imported water.

5.3 AESTHETIC RESOURCES

5.3.1 OPEN SPACE

The 17-plus acres of Neighborhood Parks will provide a healthy and scenic outdoor asset while the tree-lined Multi-Purpose Trail and agricultural buffer will provide a pleasing background for residents and the larger community.

5.3.2 ROADWAYS

New 14 to 16-foot landscaped medians will be added to Ventura Road, Doris Avenue and Patterson Road. Large, landscaped parkways (15-30 feet) will separate arterial streets from the project. A greenbelt along Beverly Drive will provide a pleasant walking or biking



connection to the community park, schools and the Mixed Use area.

5.4 CHARACTER ENHANCEMENT

The Teal Club Specific Plan seeks to create an environment where most of the services and activities for residents and employees are located within walking distance of their homes or offices. Encouraging walking requires appropriate destinations and linkages as well as the creation of a pedestrian-friendly environment. The Teal Club Specific Plan therefore incorporates traditional neighborhood design components that promote “porch and street orientation” that fosters personal interaction between private buildings and the public realm.

Courtyard/Cluster homes (R-2) include porches and architectural elements reflecting the early twentieth century diversity and character of style evident in the nearby Oxnard Historic District.

Providing nodes of activity also enhances the pedestrian experience. The neighborhood park is located at the mid-point of the main pedestrian axis through the site so that it is a visible destination for most residents.

5.5 LANDSCAPE DESIGN

Landscaping is intended to be native or adapted species in all open space areas and should need minimal irrigation after a grow-in period. Trees are an important part of the overall landscape plan and will be selected based on a number of criteria as defined in the Landscaping Design Guidelines section of this Specific Plan.

5.6 AGRICULTURE

Existing agricultural fields to the west of the project area will be buffered according to the County of Ventura Agricultural/Urban Buffer Policy revised July 19, 2006. A minimum 150-foot buffer area with a vegetative screen as defined in the county’s buffer policy will be placed between agricultural uses and the new developed uses. The Phase 2 park and Patterson Road provide the permanent west edge project buffer. The development of Phase 1 will also buffer the remaining agricultural fields located in Phase 2 of the Specific Plan until such time as Phase 2 is developed. The Phase 1 Agricultural Buffer is shown in Figure 8-1.

5.7 AIR QUALITY

In addition to standard construction mitigation measures, the land-use and zoning of the Teal Club Specific Plan co-locates a variety of uses in order to minimize vehicle miles traveled. Jobs, housing, public/semi-public uses, Urban Village commercial services (restaurants, shops, childcare, etc.), recreational opportunities and transit options will be connected by safe, attractive greenbelts, trails and bikeways. When combined with the features identified in Chapter 2 -Sustainable Community, the Teal Club Specific Plan will effectively reduce the number of vehicle miles that residents need to travel due to the availability of services and alternative modes of transportation listed above.

6

SAFETY & HAZARDS

6.1 OVERVIEW

This chapter addresses both the safety and noise factors relevant to the Teal Club Specific Plan.

6.2 AIRPORT COMPATIBILITY

The Teal Club Specific Plan is located just north of the Oxnard Municipal Airport and within the airport's Sphere of Influence. The site lies directly below the landing pattern for aircraft approaching the airport from the northwest. The Airport Air Safety Zones map and project location are shown in Figure 6-1. Therefore, the Teal Club Specific Plan is designed to meet the conditions identified in the Ventura County Comprehensive Airport Land Use Plan (ACLUP) for sites located within the Traffic Pattern Zone (TPZ):

- The structural coverage of residential units is less than 25% of the overall site area
- The structural coverage of commercial or industrial development is less than 50% of the site area.
- An aviation easement will be granted to the County of Ventura.

The land use density maximums in this section apply to the overall Specific Plan area. Each development phase shall show compliance with these overall density requirements as part of the Tentative Tract Map filing for both the proposed development and the cumulative development.

Further, the Federal Aviation Administration (FAA) has jurisdiction over development proposals under Title 14, Code of Federal Regulations, Part 77. Potential noise impacts from airport operations will be analyzed through the project's Environmental Impact Report (EIR) and mitigations implemented.

The project plans shall address landscape materials the size and location of trees to minimize any potential hazard.

6.3 NOISE

The project's location next to the Oxnard Airport requires that all buildings include noise buffering. The noise analysis and mitigations are detailed in the EIR.

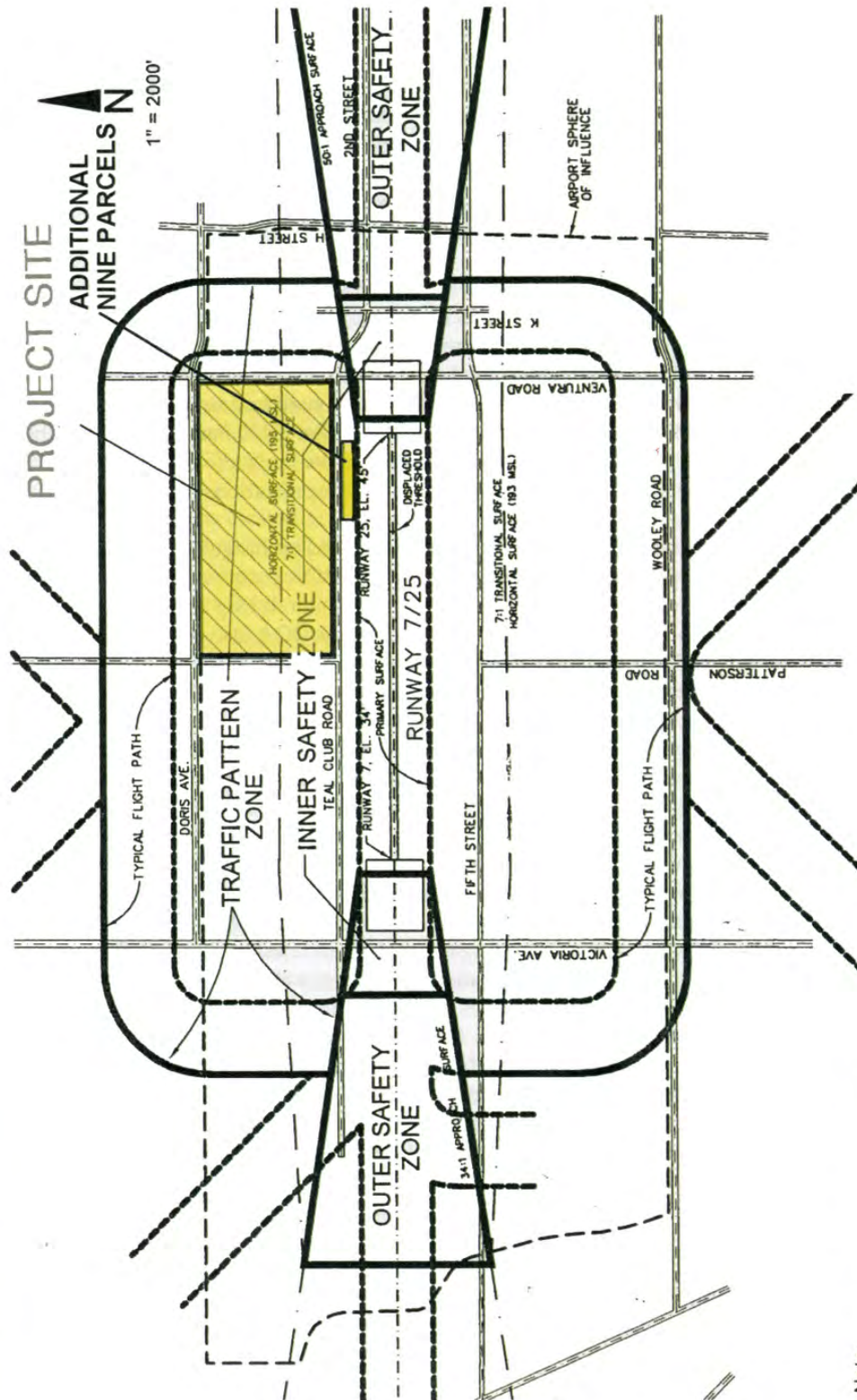
6.4 TRAFFIC & PEDESTRIAN SAFETY

The Teal Club Specific Plan circulation plan is designed to enhance the safety and comfort of all modes of transportation. Street widths have been minimized and most allow on-street parking which slows vehicular traffic. Wide sidewalks are separated from traffic with landscaped parkways and a Class I trail links much of the residential land uses to both the Commercial Mixed Use area and neighborhood park. Design guidelines require buildings to front the streets with porches or storefronts. This keeps more "eyes on the street" to further increase safety.



FIGURE 6-1 AIR SAFETY ZONES MAP

Teal Club Specific Plan EIR
Section 4.7 Hazards and Hazardous Materials



Note:
FAA Runway Protection Zone Boundary and
CLUP Inner Safety Zone Boundary are identical

Air Safety Zones
Figure 4.7-1
City of Oxnard

Source: Heliplanners

7

HOUSING & URBAN VILLAGE CONCEPT

7.1 OVERVIEW

The Teal Club Specific Plan is an important component of the implementation of the City of Oxnard 2030 General Plan, the City of Oxnard General Plan Housing Element and the State-approved Regional Housing Needs Assessment. The Specific Plan, General Plan Housing Element and the State-approved Regional Housing Needs Assessment allows for the development of 990 residential dwelling units in a variety of densities, product types and price ranges to meet the City's growing population.

7.2 URBAN VILLAGE CONCEPT

This Specific Plan is a City of Oxnard-designated Urban Village. As discussed throughout the Specific Plan, the design guidelines, housing types, mix of land uses, and landscape and pedestrian orientation concepts implement a neo-traditional town planning concept. Chapter 3 of the 2030 Oxnard General Plan addresses the vision to guide future development in the City. In

particular, Community Development Goal CD-7 calls for the “Development of vibrant mixed-use urban villages characterized by a mix of land uses, transit accessibility, pedestrian orientation, and neighborhood identity.”

General Plan Policy CD-7.1 defines the implementation of Goal CD-7 as follows:

Establishment of Urban Villages

Six areas of the City are initially designated as Urban Villages. A specific plan for each area shall be prepared in advance of implementing an Urban Village in a specific authorized location. Additional Urban Villages and guidelines may be subsequently adopted by the City Council. Urban Villages shall be characterized by:

- *Infill and/or development of formerly agricultural land*
- *Reinvestment in the existing community*
- *Mixture of land uses*
- *Mix of residential densities and housing types*
- *Providing a minimum of 15 percent affordable housing*
- *Location along or near corridors, downtown, and transit nodes*
- *Transit, pedestrian, and bicycle circulation given high priority*



Within the General Plan the Teal Club Specific Plan is further described as follows:

TEAL CLUB SPECIFIC PLAN

Location. Teal Club Road, Patterson Road, Doris Avenue, and Ventura Road.

Land Use. Transit oriented residential with supporting mixed use, schools, parks, and neighborhood commercial services.

Overview. The intent of this urban village is to encourage neo-traditional town planning compatible with surrounding uses and the Oxnard Airport with a focus on sustainability by using green building and planning principles, provision of adequate public and semi-public uses, transit-oriented development, and an identity-creating entry component facing Ventura Road. A central focus of this development will be in the provision of balanced community with jobs, school, recreation, shopping, and affordable and market-rate housing.

The Teal Club Specific Plan, as formulated within this document, implements the General Plan Urban Village Goal CD-7. The land use plan, community layout and development standards and guidelines ensure that the objective criteria of General Plan Policy CD-7.1 will be implemented upon completion of the Specific Plan. The General Plan Overview description of the Teal Club Specific Plan is also fully-realized by the implementation of this Specific Plan document. To achieve an appropriate mix of housing the Specific Plan has established density ranges for each Planning Area. These

density ranges provide for a wide variety of housing types to meet a diverse range of community housing needs. Unit types will include Medium-High Density market-rate and affordable income-restricted apartments (18 to 30 units per acre), condominium and townhome for-sale housing (12 to 18 units per acre) and detached small-lot, single-family homes (7 to 12 units per acre). Within these product types there will be a range in unit sizes to accommodate single occupancy as well as larger household groupings.

7.3 INCLUSIONARY HOUSING

As an Urban Village the Teal Club Specific Plan will provide for fifteen percent of the overall unit count to be affordable, inclusionary housing based on household income as defined by the California Health and Safety Code. Of this fifteen percent total requirement, the following percentages of income levels shall apply:

40% - Very Low Income

60% - Lower Income

The qualifying inclusionary units may be co-located in multi-family buildings or dispersed throughout the project but shall be built with a similar quality of materials and amenities so as not to be distinguishable from market-rate units.

Phase 1 of the Specific Plan shall be required to provide the inclusionary units for Phase 1 within the Phase 1 boundary. These inclusionary, affordable units shall be constructed in PA 5 and/or PA 6, as the developer may elect.

In the event that land is donated to the City or a qualified affordable housing developer within Phase 1 specifically for a 100% affordable, inclusionary housing project, that project may apply for any and all density bonus provisions provided under Local or State Law in force at the time of the affordable, inclusionary project approval. Any density bonus project approved under Local or State density bonus provisions shall be evaluated as required under the applicable California Environmental Quality Act and other Local or State laws.

Phase 2 of the Specific Plan shall be required to provide the inclusionary units for Phase 2 within the Phase 2 boundary. In the event that land is donated to the City or a qualified affordable housing developer within Phase 2 specifically for a 100% affordable, inclusionary housing project, that project may apply for any and all density bonus provisions provided under Local or State Law in force at the time of the affordable, inclusionary project approval. Any density bonus project approved under Local or State density bonus provisions shall be evaluated as required under the applicable California Environmental Quality Act and other Local or State laws.

7.4 RESIDENTIAL UNIT MIX

The mix of residential units is intended to meet existing and future community needs. The unit mix and unit sizes shall be evaluated by City staff and Planning Commissioners as part of the development review and approval process for subdivisions and individual discretionary permits within the Specific Plan. This review shall include consideration of the inclusion of very small units, multi-generational units, large households, seniors or special needs. The density ranges encourage a combination of detached, attached and multi-story units to meet the widest range of household types possible in a pedestrian-friendly and transit-accessible location.



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8

IMPLEMENTATION

8.1 PHASING

The project includes two major phases based on ownership as illustrated in Figure 8-1 and 8-2. Development of the Specific Plan is anticipated to occur over a five to ten year period. As a result, there will be a number of individual projects and sub-phases within each phase. Development of the major land use components may occur independently of one another or in parts. It is intended that agricultural production continue, to the degree economically feasible, on the undeveloped segments of the Specific Plan area. The right to farm under generally accepted standards near development is accorded to each property within the Specific Plan area.

The project developer shall construct, repair, and/or upgrade as necessary the infrastructure improvements that are within the project site and serve only the project at its sole cost and expense. The project developer shall construct, repair, and/or fund the infrastructure upgrades necessary beyond the project boundary as required to serve both the project and the city. Infrastructure reimbursement agreements shall be required between all developments within the Specific Plan to ensure an equitable, proportional sharing of backbone infrastructure costs.

The Specific Plan EIR Mitigation Monitoring and Reporting Program shall be utilized as the guiding document for providing a logical extension of necessary infrastructure to support development of individual Planning Areas within the overall project. As development occurs within the Specific Plan area onsite and offsite roadway improvements shall be provided to insure that traffic impacts within and beyond the Specific Plan area are mitigated. Other infrastructure improvements including “Advanced Water Purification” water, sewer service, storm drains and water quality treatment systems, and public utilities shall be provided to serve the needs of the individual Planning Areas and project phases.

The extent and sequence of phases of development will be approved through the City’s development review process and subdivision map approval process with implementation of the utility master plans, as contained in this Specific Plan, as required to provide necessary utility services. Each development shall be responsible for improvements within their individual project boundary as defined within the Phase 1 and Phase 2 Development Agreements and the future implementing entitlement Subdivisions, Tract Maps, and project Conditions of Approval.

8.2 AGRICULTURAL BUFFERS

Agricultural buffers are required to separate new development from existing agricultural uses. The primary agricultural buffer will be parallel to Patterson Road adjacent to Phase 2 development. It will be installed concurrently with the Patterson Drive road improvements identified in Chapter 4 - Infrastructure. The Agricultural buffer area extends into



the neighborhood Park along Patterson Road. Uses within the buffer area shall conform to the Ventura County Agricultural Commission's adopted policies.

Interim agricultural buffers may be necessary between Phase 1 and Phase 2 of the Teal Club Specific Plan. The first phase shall provide a full 150-foot structural setback buffer adjacent to any remaining agricultural uses as shown in Figure 8-1.

8.3 FINANCING DISTRICTS

At the developer's request, the City shall, pursuant to California Government Code Section 53311 et. seq., establish a Community Facilities District for capital improvements and create bonded indebtedness to finance the construction or acquisition of certain facilities such as roads, utilities, drainage, public facilities and other infrastructure.

If the City and Developer choose to, the City shall, pursuant to California Government Code Section 53311 et. seq., establish a Community Facilities District for the purpose of funding the maintenance costs of certain improvements such as roads, utilities, drainage facilities, parks, landscaping and public facilities.

8.4 DEVELOPMENT REVIEW PROCESS

All development within the Teal Club Specific Plan area is subject to the standards and guidelines in this Specific Plan. As part of the discretionary permit review process (i.e., staff review, development design review permit, Special Use Permit, subdivision map approval, etc.), the City of Oxnard shall review project submittals for consistency with

these standards and guidelines. Subdivision maps and related discretionary permits shall be processed in accordance with the procedures established by the City of Oxnard and defined within the City of Oxnard Municipal Code.

8.5 SPECIFIC PLAN ADMINISTRATION

The Specific Plan includes development and design guidelines but cannot anticipate every potential condition requiring decisions within the development review process. Any subsequent discretionary approval or amendment to the Specific Plan must be consistent with the General Plan as amended and/or updated.

Development applications that are not in substantial conformance with the Specific Plan or that may require an amendment to the Specific Plan shall be governed by the following processes under the following four application categories: Minor Modifications, Major Modifications, Specific Plan Amendments, and Special Use Permits.

8.5.1 MINOR MODIFICATIONS

The following items may deviate, within specified limitations, from the adopted Specific Plan but shall be considered to be in Substantial Conformance with the Specific Plan. This revision shall occur concurrent with the applicable discretionary permits.

FIGURE 8-1 PROPOSED PHASED IMPROVEMENT PLAN - PHASE 1

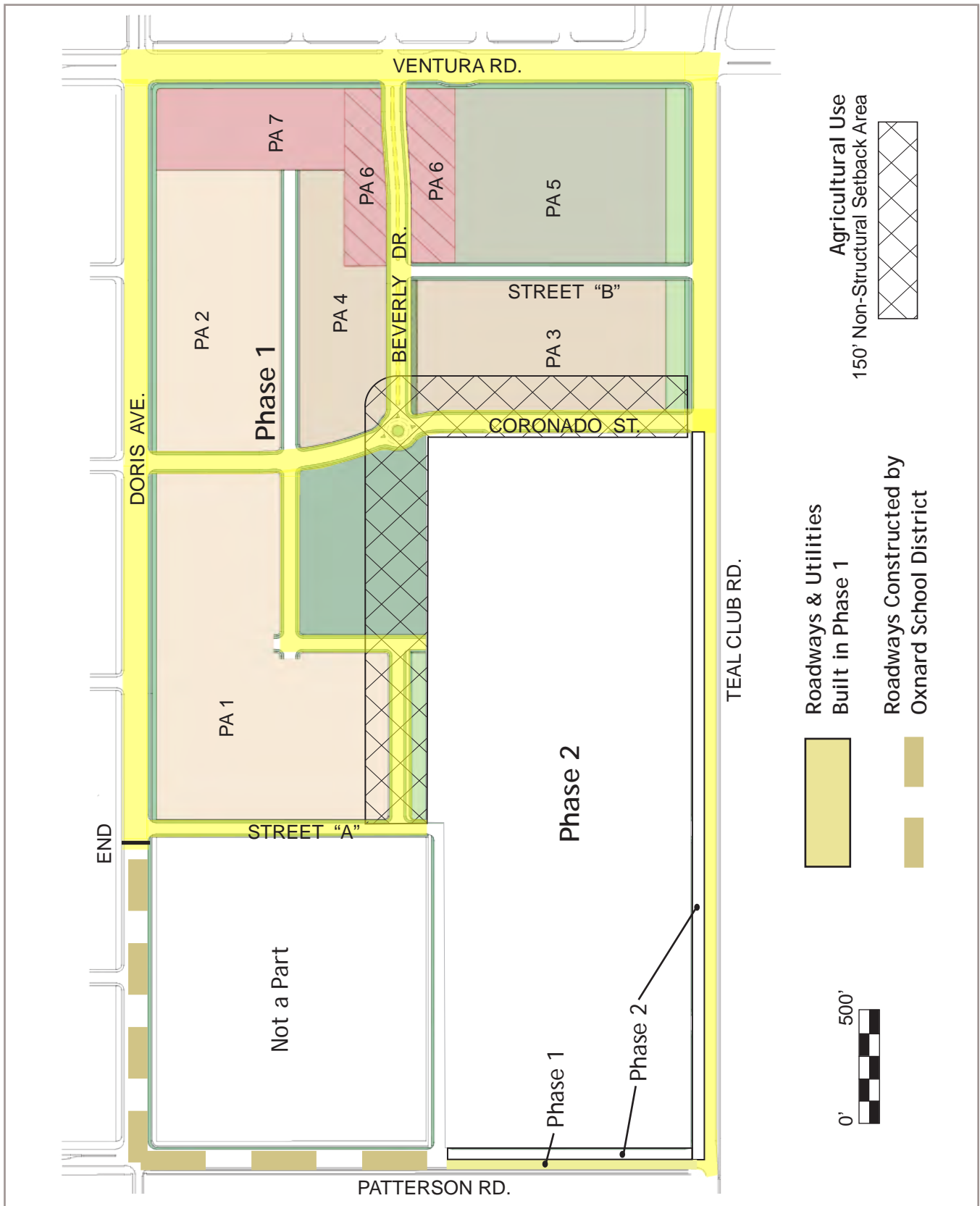
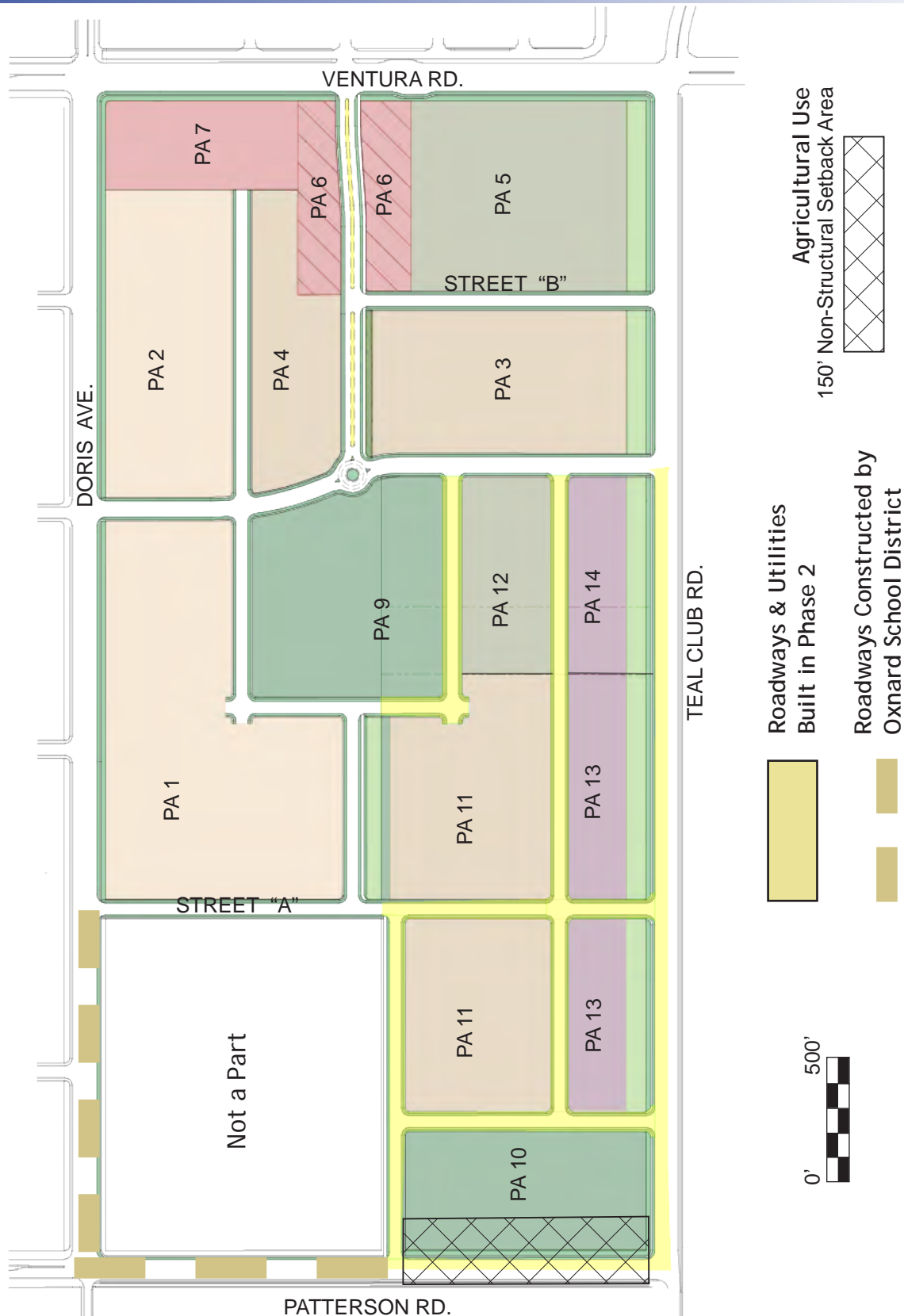




FIGURE 8-2 PROPOSED BUILD-OUT IMPROVEMENT PLAN



Examples of Minor Modifications include the following:

- ❑ Final facility sizing and alignment of water, sewer, and storm drain improvements (as directed by the City Engineer);
- ❑ Change in utility and/or infrastructure serving agency;
- ❑ Alignment of streets within neighborhoods so long as the number and approximate spacing of connections is in general conformance with the Specific Plan Land Use Plan;
- ❑ Local street, road, and multi-purpose trail alignments provided that connecting of destination points is maintained;
- ❑ Minor landscape and streetscape design modifications consistent with the Design Guidelines contained in this document and approved through the development review process;
- ❑ Deletion of utility infrastructure deemed unnecessary by the City Engineer.
- ❑ Re-distribution of dwelling units between Planning Areas provided that the re-distribution falls with the allowed density for each affected Planning Area.

8.5.2 SPECIFIC PLAN AMENDMENTS

Amendments may be requested at any time pursuant to Section 65453(a) of the California Government Code. Any proposed modification to the Specific Plan beyond the defined limits of the Minor or Major Modification will be processed as a Specific Plan Amendment. Depending upon the nature of the proposed Specific Plan Amendment, additional environmental analysis may be required, pursuant to Section 15162 of the California Environmental Quality Act.

8.6 MITIGATION MONITORING & REPORTING PROGRAM

Refer to the Teal Club Specific Plan EIR Mitigation Monitoring and Reporting Program for implementation and reporting of the Mitigation Measures.



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9

GENERAL PLAN CONSISTENCY

The Teal Club Specific Plan is a defined Urban Village and Specific Plan area within the City of Oxnard General Plan. The California Government Code (Sections 65450 through 65457) permits the adoption and administration of Specific Plans as an implementation tool for elements contained in the local General Plan. As a result, Specific Plans must demonstrate consistency in regulations, guidelines and programs with the goals and policies set forth in the General Plan.

The City of Oxnard 2030 General Plan contains the following elements that relate to the consistency analysis for the Teal Club Specific Plan: Sustainable Community, Community Development, Infrastructure & Community Services, Environmental Resources, Safety & Hazards and Housing.

An exhaustive review and analysis of each of these elements has been conducted as a part of the Teal Club Specific Plan Draft Environmental Impact Report (DEIR). The Teal Club Specific Plan DEIR General Plan Consistency Analysis was prepared by Rincon Consultants under the direction of the City of Oxnard.

The following pages list the applicable City of Oxnard 2030 General Plan Policies and provide a discussion of the project impacts, mitigations and consistency upon implementation of the Specific Plan and Final Environmental Impact Report (FEIR) mitigation measures. In each case the DEIR analysis confirms that the Teal Club Specific Plan is either deemed Consistent with the applicable General Plan Policy or Potentially Consistent with the applicable General Plan Policy, subject to final confirmation by conformance with the mitigations and requirements to be set forth in the FEIR.

The Teal Club Specific Plan, located entirely within the City of Oxnard's established City Urban Restriction Boundary (CURB), has been identified in the approved City of Oxnard 2030 General Plan as one of the last remaining major parcels for development within the ultimate City of Oxnard boundaries. Upon annexation to the City of Oxnard and subsequent completion of the infrastructure improvements outlined within the Teal Club Specific Plan and FEIR Mitigation Measures there would be no significant impacts to public services within the City of Oxnard.

The Teal Club Specific Plan mix of residential, commercial, industrial, recreational and transit connections will contribute to the overall economic and cultural vitality of the City of Oxnard. The City of Oxnard will receive economic benefits from the tax-increment revenue derived from the development of the Specific Plan area. The Specific Plan shall provide for self-funding of internal community infrastructure through the formation of a Maintenance Community Facilities District (CFD) or Community Service Area, subject to approval by the City of Oxnard.

Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>SUSTAINABLE COMMUNITY</i>	
<i>SC-3.1. New Residential Development. Encourage incorporation of passive and active energy and resources conservation design and devices in new residential development and substantial remodels and/or expansions.</i>	<u>Level II - Consistent</u> Mitigation Measures AQ-2(b) and AQ-2(c) require that construction and building management contracts for residential development within the TCSP include energy saving requirements mandated by State Green Building Code, as amended during the life of the project. Mitigation AQ-2(c) also required that all structures include passive energy conservation design elements, including building material massing, orientation, architectural elements (deeply recessed windows, eave overhangs, etc.), landscape shading, or low-impact materials, window glazing to increase insulation, and water circulation pumps to reduce water use, and/or similar measures shown to be equally effective. This mitigation measure that applies to all development proposed within the TCSP and within the nine parcels proposed for Annexation.
<i>SC-3.8. Require Use of Passive Energy Conservation Design. As part of the City and Community EAP's, require the use of passive energy conservation by building material massing, orientation, landscape shading, materials, and other techniques as part of the design of local buildings, where feasible.</i>	
<i>SC-3.12. Encourage Natural Ventilation Review and revise applicable planning and building policies and regulations to promote use of natural ventilation in new construction and major additions or remodeling consistent with Oxnard's temperate climate.</i>	<u>Level II - Consistent</u> Mitigation Measure AQ-2(e) requires all applicants for all projects within the TCSP area and within the nine parcels proposed for Annexation to include natural ventilation in building design plans whenever feasible, as required by the State Green Building Code, as amended during the life of the project.
<i>COMMUNITY DEVELOPMENT</i>	
<i>CD 1.4. Transportation Choices. Promote the application of land use and community designs that provide residents with the opportunity for a variety of transportation choices (pedestrian, bicycle, transit, automobile).</i>	<u>Level I - Consistent</u> The TCSP consists of traditional neighborhood design components that promote “porch and street orientation” and encourage walking and interaction between residents. Medium and Medium-High Density areas would orient to internal pathways and common areas with connection to the public walking network and to the Urban Village. The Urban Village would be oriented around a public plaza, and would be within walking distance of residential units and readily accessible to a new bus stop on Ventura Road (Section 2.0, <i>Project Description</i>).



Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>CD 1.5. Housing Variety. Promote the development of a variety of housing types throughout the City including apartments, condominiums, lofts, townhouses, and attached and detached single family units.</i>	<u>Level I - Consistent.</u> The TCSP envisions development of up to 990 residential dwelling units in a variety of densities and product types including both market-rate and affordable housing. In addition to single-family residential units, the TCSP includes single-family courtyard homes, single-family townhomes and multi-family condominiums and apartments (Section 2.0, Project Description).
<i>CD 1.6. Public Facilities. Enhance resident quality of life by providing adequate space for schools, libraries, parks and recreation areas, as well as space for the expansion of public facilities to support the community's vision.</i>	<u>Level I - Consistent.</u> The TCSP includes a 6.5-acre public (City) Community Park (PA 8) with playground equipment, picnic tables, restrooms, and backstops and fencing for softball/baseball play and soccer use. Within the residential and commercial PA's, a 6.5-acre community park, a 3.5-acre community park, and a 7.4-acre community park combine for a total of 17.4 park and open space acres. Combined with the Beverly Dr. greenbelt, the TCSP would provide 17.8 gross acres of public parks and open space, about 12% of the project acreage.
<i>CD 1.7. Compact Development. Promote the use of development patterns that are more compactly built and use space in an efficient aesthetic manner as part of the community vision.</i>	<u>Level I - Consistent.</u> High-density residential zones are 30 or more dwelling units per acre and medium-high zones are 18 to 30 dwelling units per acre, as described in the City of Oxnard General Plan 2030. The TCSP would include maximum densities of between 10 and 30 dwelling units per acre. The general distribution of densities for the site is included in the 2030 General Plan Land Use map, and the proposed TCSP generally adheres to that distribution.
<i>CD 1.8. Natural Resource Conservation. Promote a high quality of life within the community, incorporating the retention of natural open space areas, greenbelts, and the provision of adequate recreational facilities.</i>	<u>Level I - Consistent.</u> As discussed in Section 4.4, Biological Resources, the project area does not include any natural open space areas, as it is currently used for agricultural activities and urban development. The project area is adjacent to agricultural areas to the west that are outside of the CURB boundary. Approximately 17.76 acres of open space and parks are proposed within the TCSP area. As discussed in Section 4.12 Public Services and Recreation, this acreage would exceed the required 12 acres of parkland for the project.

Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>CD 1.9. Commute Reduction. Minimize the commuting distances between residential concentrations and employment centers by encouraging the development of mixed land uses in appropriate areas.</i>	<u>Level I - Consistent.</u> Medium and Medium-High Density areas in the TCSP would orient to internal pathways and common areas with connection to the public walking network, the proposed Business Research Park zone (PA 13 and 14), the proposed commercial zones (PA 6 and 7), and the nine parcels south of Teal Club Road which would be rezoned for employment-generating uses.
<i>CD 1.10. Jobs-Housing Balance. Consider the effects of land use proposals and decisions on efforts to maintain an appropriate jobs-housing balance ratio.</i>	<u>Level I - Consistent.</u> The TCSP includes residential uses, as well as retail, office, and a business and research park, which would offer job opportunities to residents. The adopted VCOG 2040 forecast projects a total of 83,328 jobs and 71,602 households for the City of Oxnard by the year 2040. Therefore, the 2040 jobs/housing ratio would be 1.16:1 which is within the range of 1.1 and 1.34 jobs per housing unit, the acceptable jobs/housing ratio range identified by the VCOG (VCOG, May 2008). With the additional 2,651 jobs and net increase of 988 housing units under the proposed Specific Plan, the jobs/housing would change to 1.18:1 (more skewed towards a greater number of jobs than housing units) but would still be within the acceptable jobs/housing ratio. Therefore, the project would not move the City's ratio out of the VCOG range.
<i>CD 1.12. Avoiding Encroaching the Oxnard Airport. Retain land within the airport hazard area as permanent open space as shown on the Land Use Map or otherwise recommended by the County Department of Airports.</i>	<u>Level II - Consistent.</u> Development of the Specific Plan would place residential and commercial uses within 2,000 feet of the Oxnard Airport runway, potentially exposing people residing and working in the area to safety hazards. However, no habitable development is proposed within the airport's inner or outer safety zone, and as discussed in Section 4.7, Hazards and Hazardous Materials, the probability of an accident occurring in the Plan Area is low. Further, the presence of nearby emergency landing areas would reduce accident hazards. Safety would be further ensured through mitigation measures HAZ-5(a-c). The City of Oxnard has referred the proposed project to the ALUC for review and comment. The ALUC has determined that the Project is consistent with the Airport Comprehensive Land Use Plan. The City must consider the comments of the ALUC prior to making a decision on adoption of the Specific Plan.



Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>CD 3.1. Neighborhood Preservation. Protect existing residential neighborhoods from the encroachment of incompatible activities and land uses as determined through environmental review and/or determination by the Planning Commission.</i>	<u>Level II - Consistent.</u> The development within the TCSP would be compatible with the existing residential neighborhood located to the north of the TCSP area. The proposed Urban Village would locate residential uses adjacent to the existing residences. Residences to the east are across Ventura Road from the project area, which would provide a buffer from the neighborhood commercial and higher density residential uses proposed in the eastern portion of the TCSP area.
<i>CD 4.1. Mitigate Land Use Conflicts. Mitigate conflicts between commercial and other land uses, especially residential and recreational uses.</i>	<u>Level II - Consistent.</u> Proposed commercial uses in the TCSP would not be located directly adjacent to any existing residential or recreational uses. Existing residences to the east of proposed commercial land use in PA 6 and 7 are across Ventura Road from the project area, which would provide a buffer from the neighborhood commercial uses.
<i>CD 5.1. Industrial Clustering. Encourage the clustering of industrial uses into areas that have common needs and are compatible in order to maximize their efficiency.</i>	<u>Level II - Consistent.</u> The nine parcels (11.4 acres combined) to be Annexed south of Teal Club Road are a mix of vacant land and existing small residential and industrial development. Upon Annexation, these nine parcels would be zoned Light Manufacturing (M-1) by the City of Oxnard and would encourage the clustering of industrial uses. The airport area is designated by the City as an area where industrial uses are allowed. Business and Research Park uses are also proposed for the TCSP area nearest to the airport (PA 13 and 14).
<i>CD 5.2. Compatible Land Use. Ensure adequate separation between sensitive land uses (residential, educational, open space, healthcare) to minimize land use incompatibility associated with noise, odors, and air pollutant emissions.</i>	<u>Level II - Consistent.</u> The proposed industrial component of the Specific Plan would be compatible with existing industrial uses as well as the adjacent agricultural, open space, and residential land uses with incorporation of the mitigation measures specified in Section 4.2, Agriculture. See Section 4.7, Hazards and Hazardous Materials and Section 4.10, Noise for further discussion regarding compatibility. The proposed Business and Research Park uses in PA 13 and 14 would not include heavy manufacturing uses likely to produce incompatible odors.

Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>CD 5.3. Available Services. Encourage industrial activities to locate where municipal services are available including adequate storm drainage and water facilities, as well as easy access to multiple modes of transportation.</i>	Level I - Consistent. The nine parcels (11.4 acres combined) to be Annexed south of Teal Club Road are a mix of vacant land and existing small residential and industrial development. Municipal services currently exist in this area including water and sewer lines. When projects are proposed within the nine parcels and in the TCSP area, they would be required to comply with this 2030 General Plan policy and with existing regulations for stormwater. The project area is accessible to bicycle lanes and bus stops.
<i>CD 5.5. "Green" Major Transportation Routes. Guide industrial development to locate near transportation facilities capable of handling goods movements in an efficient manner without decreasing the level of service on the transportation network or dividing existing neighborhoods.</i>	Level I - Consistent. Industrial development would be located in the vicinity of major transportation routes including Ventura Road, Fifth Street and Victoria Avenue, and would not divide an existing neighborhood, as discussed above under Impact LU-1. The nine parcels south of Teal Club Road proposed for Annexation, which could be rezoned for industrial uses, are located adjacent to the Oxnard Airport, generally surrounded by other industrial land uses and commercial development, and no existing neighborhood is located on either side of the parcels. The parcels are located within 2.5 miles of Highway 101, a major transportation route.



Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>CD 6.1. Agricultural Buffers. Require that agricultural land uses designated for long-term protection and production be buffered from urban land uses through the use of techniques including, but not limited to, greenbelts, open space setbacks, fencing, berming, and windrows.</i>	<u>Level II - Consistent.</u> As described in Section 4.2, Agricultural Resources, and Mitigation Measure AG-2, which requires interim agricultural buffers, Phase 1 would include 91.83 acres of the site and interim agricultural buffers are proposed to allow Phase 2 owners to continue farming indefinitely. Phase 1 would also include all internal roadway circulation needed to service Phase 1. Proposed agricultural buffers would be 300 feet or 150 feet wide with a double row of appropriate trees (windrows). All proposed Phase 1 roads would be built and operable; residences for lots in the Phase 1 area within 150-foot buffers would not be built until Phase 2 is committed to development. Trespassing, vandalism, and pilferage impacts would be reduced through the development of the Community Park separating agricultural production and proposed urban land uses. Agricultural uses to the west would be buffered by open space planning areas and North Patterson Road. Finally, the community park (PA 10) would provide a buffer between the residential and business park use and agricultural use to the west.
<i>CD 6.2. Agricultural Preservation. Preserve agricultural land and uses within the Oxnard Planning Area unless other uses are allowed through a future CURB amendment and/or applicable exemptions.</i>	<u>Level II - Consistent.</u> When voters approved the city's Save Open Space and Agricultural Resources ("SOAR") initiative in 1998, the Teal Club site was located inside the urban growth boundary ("CURB").

Table 9-1
2030 General Plan Policy Consistency

General Plan Policy	Discussion
<p><i>CD-7.1 Establishment of Urban Villages: Six areas of the City are initially designated as Urban Villages. It is the intent of the Urban Village designation that specific or strategic plans for each area will be prepared in advance of the planning entitlement process. Additional Urban Villages and guidelines may be subsequently adopted by the City Council*. Urban Villages are envisioned as characterized by:</i></p> <ul style="list-style-type: none"> <i>• Infill and/or development of formerly agricultural land</i> <i>• Reinvestment in the existing community</i> <i>• Mixture of land uses</i> <i>• Mix of residential densities and housing types</i> <i>• Providing a minimum of 15 percent affordable housing</i> <i>• Location along or near corridors, downtown, and transit nodes</i> <i>• Transit, pedestrian, and bicycle circulation given high priority</i> <p><i>Teal Club Specific Plan:</i></p> <ul style="list-style-type: none"> <i>• Location. Teal Club Road, Patterson Road, Doris Avenue, and Ventura Road.</i> <i>• Land Use. Transit oriented residential with supporting mixed use, schools, parks, and neighborhood commercial services.</i> <i>• Overview. The intent of this urban village is to encourage neotraditional town planning compatible with surrounding uses and the Oxnard Airport with a focus on sustainability by using green building and planning principles, provision of adequate public semi-public uses, transit-oriented development, and an identity creating entry component facing Ventura Road. A central balanced community with jobs, school, recreation, shopping, and affordable and market-rate housing.</i> 	<p><u>Level I - Consistent.</u> The proposed TCSP would be consistent with the 2030 General Plan's Urban Village policy, as it is one of the areas specifically defined as an Urban Village. The TCSP would provide a mix of land uses including but not limited to commercial, retail, business research, residential, public/semipublic uses, and open space.</p> <p>The TCSP envisions development of up to 990 residential dwelling units in a variety of densities and product types including both market-rate and affordable housing (15% affordable). In addition to single-family residential units, the TCSP includes single-family courtyard homes, single-family townhomes and multi-family condominiums and apartments (Section 2.0, Project Description).</p> <p>The TCSP would be located within 2.5 miles of Highway 101, a major transportation route and would provide access to other public transportation options, including Gold Coast Transit buses.</p> <p>The Urban Village would be oriented around a public plaza, and would be within walking distance of residential units and readily accessible to bus pull-out locations on southbound and northbound lanes of Ventura Road which would be built as part of the project. The planned bus stops would serve the project area and would help provide public transit options. Leasing preferences and incentives may be required so that the Urban Village provides a mix of uses that reduce vehicle trips by residents (Section 2.0, Project Description).</p>



Table 9-1
2030 General Plan Policy Consistency

General Plan Policy	Discussion
<i>CD-7.5 Pedestrian and Transit Scale. Design urban village areas to be pedestrian-oriented and transit accessible, incorporating block patterns, walking routes and edges, social orientation of buildings, and streetscapes to provide ease of walking and safety.</i>	<u>Level I - Consistent.</u> The urban village would be oriented around a public plaza, and would be within walking distance of residential units and readily accessible to bus pull-out locations on southbound and northbound lanes of Ventura Road, which would be built as part of the project. The planned bus stops would serve the project area and would help provide public transit options. One of the project objectives is to create an integrated vehicular, pedestrian and bicycle circulation system that connects residential, industrial, commercial and institutional uses within the project area. The proposed project would involve pedestrian, bicycle, and transit improvements as discussed in Section 2.0, Project Description, and shown in Figure 2-4.
<i>CD-7.6 Connectivity. Provide connectivity to other activity nodes in the form of roadways, transit connections, and bicycle and pedestrian linkages that encourages non-vehicular travel modes. Urban villages should be considered major transit transfer points and have amenities oriented towards transit users.</i>	
<i>CD-7.7 Urban Village Streetscapes and Identification. Include streetscape and signage programs in roadway improvements that provide each area a unique identification and enhance the functionality and beauty of entry corridors. Ensure that planned roadway improvements do not conflict with other policies that encourage pedestrian activities and circulation.</i>	<u>Level II - Consistent.</u> Development under the TCSP, when proposed, would be required to adhere to streetscape and identification programs as defined by the 2030 General Plan.
<i>CD-7.8 Road Design. Reflect the residential and commercial activities of the urban village area by using appropriate roadway widths for road and streetscape design.</i>	<u>Level II - Consistent.</u> Development under the Specific Plan, when proposed, would be required to adhere to road and streetscape design as defined by the 2030 General Plan.
<i>CD-7.9 Infrastructure Compatibility. Ensure new development within each urban village complies with the City's adopted infrastructure master plans and provides fair share contributions towards existing and future improvements necessary to serve the development.</i>	<u>Level II - Consistent.</u> As described in Section 4.14, Utilities and Service Systems, developers of the proposed project would pay fees for the necessary wastewater infrastructure, project developers would design on-site water systems to serve the development, and the proposed project would not significantly impact water or solid waste infrastructure.

Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>CD-7.11 Urban Village Open Space Areas/Parks. Park sizes and locations shall follow City standards and be within walking distance of a majority of the population.</i>	<u>Level II - Consistent.</u> The TCSP would include 17.8 acres of parks and open space, most of which would be located east of Patterson Road. The 7.4-acre park adjacent to Patterson Road would be within walking distance of all TCSP residents, as well as existing residential neighborhoods north and east of the project area.
<i>CD-7.12 Urban Village Collocation with Schools. Promote the collocation of parks with school facilities for the purpose of enhancing available open space and recreation.</i>	<u>Level II - Consistent.</u> The proposed TCSP does not involve collocation of schools and parks; however, the 7.4-acre City Community Park proposed to be located alongside Patterson Road would be near the proposed OSD school site adjacent to the TCSP area.
<i>CD-7.13 Urban Village Trail and Open Space Connections. Include trails (pedestrian and bicycle) and open space areas, where feasible within urban village areas. These facilities shall create a network that links urban villages and other neighborhoods to each other.</i>	<u>Level II - Consistent.</u> The TCSP would include open space areas, including greenbelts along Beverly Drive and Street C. The area adjacent to Patterson Road, nearest to the agricultural uses, would be developed as a 7.4 acre park area, which would be accessible to other neighborhoods.
<i>CD 8.1. Limiting Development. Continue to limit development to those areas that can be served by existing or planned utilities, transportation, and service systems.</i>	<u>Level II - Consistent.</u> See Section 4.14, Utilities and Service Systems. The proposed project is at the edge of the existing service area. It would involve development in an area that, with the mitigation proposed, can be served by existing or planned service systems. Since service does not exist currently, the project is potentially consistent.
<i>CD 8.2. Services. Continue to ensure that public services and facilities are in place at the time of need or prior to the time new development occurs in order to avoid overloading existing urban service systems.</i>	



Table 9-1
2030 General Plan Policy Consistency

General Plan Policy	
<i>CD 8.5. Impact Mitigation. Ensure that new development avoids or mitigates impacts on air quality, traffic congestion, noise, and environmental resources to the maximum extent feasible.</i>	<u>Level I - Consistent.</u> See Section 4.3, Air Quality for mitigation measures including energy efficiency design elements and construction requirements that would reduce impacts to air quality to a less than significant level. See Section 4.10, Noise, which includes mitigation measures and concludes that impacts related to noise would be less than significant. See Section 4.13, Transportation and Traffic for mitigation measures related to intersections that would ensure that impacts related to traffic would be less than significant. Other mitigation measures and impacts to environmental resources are discussed throughout this EIR and impacts would be reduced to the extent feasible.
<i>CD 8.7. Community Balance. Create an appropriate balance between urban development and preservation of agricultural uses by promoting development within the CURB while designating land outside the CURB as Resource Protection, Open Space or Agricultural land use, unless otherwise allowed through a CURB amendment and/or exemptions from the SOAR ordinance.</i>	<u>Level II - Consistent.</u> When voters approved the city's Save Open Space and Agricultural Resources ("SOAR") initiative in 1998, the Teal Club site was specifically located inside the urban growth boundary ("CURB") (Specific Plan, July 2013).
<i>CD 8.8. Public Facility Service Areas. Provide appropriate service areas for existing and planned public facilities such as a museum, secondary and elementary schools, fire stations, branch libraries, community centers, parks, and infrastructure utility for support facilities.</i>	<u>Level I - Consistent.</u> Within the residential and commercial Planning Areas are a 6.5-acre community park, a 3.5-acre community park, and a 0.4-acre greenbelt combine for a total of 10.4 park acres. A 7.4-acre City Community Park would also be included in the TCSP to the east of Patterson Road. Public access to the Community Park would be available to all Oxnard residents via Patterson Road. In addition, the property at 1618 Doris Avenue in the TCSP area, which is also within 1/4-mile of the proposed Oxnard School District school site

Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>CD 8.9. Jobs/Housing Balance & Sustainable Communities Strategy (SB 375). Incorporate inter- and intra-city jobs/housing balance in the development of the regional and subregional Sustainable Communities Strategy (SB 375), Urban Village strategy and strategic plans, with the main intent to reduce single-occupancy work-related vehicular trips.</i>	<u>Level I - Consistent.</u> The TCSP would create 2,651 employment opportunities and 988 residential units within a transit-oriented development community. This would potentially reduce single-occupancy work-related vehicular trips by locating employment centers near residences.
<i>CD 8.10. Timing of Large-Scale Development. Consider at an early stage the infrastructure investment needs of largescale developments in order to evaluate these needs as part of long-range water supply, conveyance, wastewater, and other relevant planning.</i>	<u>Level II - Consistent.</u> As described in Section 4.14, Utilities and Service Systems, existing water supply and solid waste conveyance systems would be able to serve the proposed project. Wastewater conveyance systems and an on-site Advanced Water Purification water system would be developed prior to occupancy.
<i>CD 9.4. View Corridor. Preservation. Ensure all public and private investments positively contribute to the overall character of the City by minimizing impacts on important view corridors by creating edge treatments along greenbelt areas and a landscaped buffer corridor of at least 30 feet along designated scenic corridors and other major transportation corridors.</i>	<u>Level II - Consistent.</u> Given the limited extent to which the proposed project would affect scenic vistas (see Section 4.1, Aesthetics), the project would not have a significant adverse impact on scenic vistas.
<i>CD 9.5. Unique Character Preservation. Ensure that new public and private investment maintains the unique coastal and agricultural character of the City.</i>	<u>Level II - Consistent.</u> As discussed in Section 4.2, Agricultural Resources, there was a significant and unmitigable impact associated with the conversion of agricultural land in the 2030 General Plan.
<i>CD 10.1. Human-Scale Development. In the evaluation of development proposals, require urban development on a human scale, by emphasizing the pedestrian experience over the movement and storage of vehicles.</i>	<u>Level II - Consistent.</u> The TCSP would provide for the pedestrian experience. Medium and Medium-High Density residential areas would orient to internal pathways and common areas with connection to the public walking network and to the urban village component.



Table 9-1
2030 General Plan Policy Consistency

General Plan Policy	Discussion
<i>CD 10.2. Neighborhood Themes. In the evaluation of development proposals, require neighborhood themes and principles of design, such as neotraditional town planning, which include central parks, schools, and community and commercial facilities, strong pedestrian orientation and de-emphasis of automobile related elements in new development projects.</i>	<u>Level II - Consistent.</u> The TCSP would emphasize the neighborhood themes. Medium and Medium-High Density areas would orient to internal pathways and common areas with connection to the public walking network and to the urban village. Small parks or “greenbelts” would be located along major roadways within the TCSP area, including on Beverly Drive and Street C. The TCSP also includes a Community park.
<i>CD 11.3. Protect and Enhance Cultural Resources. Ensure that new public and private investment protects and enhances Oxnard’s existing cultural resources, traditional neighborhoods, and historic districts, to the extent feasible.</i>	<u>Level II - Consistent.</u> See Section 6.o, Effects Found Not to be Significant. There are no officially designated historic resources or known cultural resources on or adjacent to the project area.

INFRASTRUCTURE AND COMMUNITY SERVICES

<i>ICS 1.2. Development Impacts to Existing Infrastructure. Review development proposals for their impacts on infrastructure (e.g., sewer, water, fire stations, libraries, streets) and require appropriate mitigation measures to ensure that proposed developments do not create substantial adverse impacts on existing infrastructure and that the necessary infrastructure will be in place to support the development.</i>	<u>Level II - Consistent.</u> The proposed project includes payment of wastewater infrastructure fees to ensure that infrastructure would be in place to support development. The project also includes development of on-site water systems. The project area is on the edge of the existing service area for utilities and water and sewer lines are located near the project area. The applicant would also be required to fund development of an additional fire station, fire engine, and staff to provide fire/emergency services to the project area (Mitigation Measure PS-1). See Section 4.12, Public Services and Section 4.14, Utilities and Service Systems for a discussion of existing and proposed infrastructure.
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Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>CIRCULATION</i>	
<i>ICS 2.5. Mitigate Impacts on County Roads. Require new development to contribute to the enhancement of Ventura County-maintained roads based on an updated City/ County Memorandum of Understanding.</i>	<i>Level II - Consistent.</i> Intersection improvements and payment of standard traffic impact fees would be required as described in detail in Section 4.13, Transportation and Traffic.
<i>ICS 3.3. New Development Level of Service C. Determine as part of the development review and approval process that intersections associated with new development operate at a level of service of "C" or better. The City Council may allow an exception to level of service "D" in order to avoid impacting private homes and/or businesses, avoid adverse environmental impacts, or preserve or enhance aesthetic integrity.</i>	<i>Level II - Consistent.</i> The proposed project would include improvements to ensure that the project does not result in a level of service lower than "C." See Section 4.13, Transportation and Traffic and mitigation measures T-1(a-h) and T-2(a-h) for specific details regarding the affected roadways and intersections.
<i>ICS 6.1. Transit Facilities for New Developments. Include transit facilities such as bus benches, shelters, pads or turnouts, where appropriate, in new development improvement plans.</i>	<i>Level I - Consistent.</i> Enhanced bus facilities on Ventura Road are included in the proposed plan. This would include multiple bus pull-out locations and bus shelters on southbound and northbound lanes of Ventura Road, which would be built as part of the project. The planned bus stops would serve the project area and would help provide public transit options.
<i>ICS 7.3. Travel Patterns. Promote compact, mixed use development patterns that compliment and encourage TDM programs, pedestrian and bicycle travel, and transit use.</i>	<i>Level II - Consistent.</i> A business/research park on the south side of the TCSP area would provide jobs within walking distance of area residents and the urban village would be oriented around a public plaza, and be within walking distance of residential units, as well as readily accessible to a new bus stop on Ventura Road. Fees would be paid to a TDM program, as described in Section 4.3, Air Quality Mitigation Measure AQ-2(a). The proposed density would be relatively compact for airport-adjacent areas.



Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>ICS 8.4. New Development Requires Bicycle Improvements. Where designated, require proposed developments to include bicycle paths and / or lanes in their plan and to clearly indicate possible bicycling hazards such as speed bumps and storm drain inlet grates in parking lots.</i>	<u>Level I - Consistent</u> . When submitted to the City, bike path design and alignment would be reviewed for appropriate safety elements. (See Figure 2-4 in Section 2.0, Project Description, for proposed pedestrian, bicycle, and transit improvements).
<i>ICS 10.2. Oxnard Airport Compatible Land Use. Continue to ensure that the land use and zoning adjacent to Oxnard Airport is compatible in order to minimize potential noise and safety problems.</i>	<u>Level II - Consistent</u> . Only a small portion along the southern boundary in the western half of the TCSP area would be within the 60 dBA CNEL contour. This portion is proposed for a retention basin and business park uses, which would be compatible with potential noise from the Oxnard Airport. The light manufacturing uses that could be located on the nine parcels south of the TCSP area would be compatible with the noise levels associated with the Oxnard Airport (see Section 4.10, Noise). The proposed TCSP area and additional Annexation area are both within the Oxnard Airport's traffic pattern zone (TPZ) and are subject to height restrictions. Development of the Specific Plan would place residential and commercial uses within 2,000 feet of the Oxnard Airport runway, potentially exposing people residing and working in the area to safety hazards. However, the probability of an accident occurring in the Plan Area is low. Further, the presence of nearby emergency landing areas would reduce accident hazards. Safety would be further ensured through mitigation measures HAZ-5(a-c). See Section 4.7, Hazards and Hazardous Materials for further discussion.

Table 9-1
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<i>General Plan Policy</i>	<i>Discussion</i>
<i>ICS 11.6. Water Conservation and/or Recycling Connection as Mitigation. Require the use of water conservation offset measures (efficient low flow fixtures and irrigation systems, drought tolerant landscaping, leak detection programs, water audits, and public awareness and education programs) and/or proportional contributions to Advanced Water Purification water production and/or conveyance infrastructure related to the GREAT Program as mitigation for water supply shortage as determined by a Water Supply Assessment, CEQA documentation, or similar analysis as part of new or master plan development review.</i>	<u>Level II - Consistent.</u> TCSP buildout is proposed to be “water neutral” so that future water demand would not exceed the proposed transfer of water rights. To provide adequate potable water for the TCSP project, the existing agricultural water rights within the TCSP area would be transferred for municipal and industrial uses to the City of Oxnard. All TCSP development water needs for which Advanced Water Purification water use is appropriate would be connected to the City’s Ventura Road Advanced Water Purification water distribution pipeline. Advanced Water Purification water would be used, at a minimum, for all landscape irrigation and other water conservation measures would be required by mitigation measures UTIL-3(c) and UTIL-3(d). See Section 4.14, Utilities and Service Systems for further discussion of water conveyance.
<i>ICS 11.7. Water Wise Landscapes. Promote water conservation in landscaping for public facilities and streetscapes, residential, commercial and industrial facilities and require new developments to incorporate water conserving fixtures (low water usage) and water-efficient plants into new and replacement landscaping.</i>	<u>Level II - Consistent.</u> Advanced Water Purification water would be used, at a minimum, for all landscape irrigation, provided that the Utilities Department extends the Advanced Water Purification water facilities an extension of services to the project area is approved. Development within the TCSP area and the nine Annexed parcels would be required to adhere to Oxnard City Code (OCC) Chapter 22 Water, Section 22-243 Compliance Requirements, which requires that the landscape area of projects proposing commercial or industrial uses shall be designed without the use of turf and with 100% water wise plants. The landscape area of single-family residential, and multi-family residential projects shall be designed with no more than 40% of the landscaped area in turf or plants that are not water wise plants. Mitigation Measure AQ-2(d) requires implementation of passive energy conservation design techniques such as water conserving fixtures; other water conservation measures would be required by mitigation measure UTIL-3(c) for exterior water conservation.
<i>ICS 11.12. Water for Irrigation. Require the use of non-potable water supplies for irrigation of landscape and agriculture, whenever available.</i>	<u>Level II - Consistent.</u> Advanced Water Purification water would be used, at a minimum, for all landscape irrigation, provided that the Utilities Department extends the Advanced Water Purification water facilities to the project area, as determined in the Teal Club FEIR.



Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>ICS 13.2. Adequate Storm Drains and NPDES Discharge Treatment. Provide storm drainage facilities with sufficient capacity to protect the public and property from the appropriate storm event and strive to meet storm water quality discharge targets set by NPDES and related regulations.</i>	<u>Level II - Consistent.</u> The site would generally drain into new storm drains within Teal Club Road and Patterson Road, with additional storm water management provided by the proposed retention and on-site infiltration areas shown on the Land Use Plan (Figure 2-3) as “storm water treatment” areas. The precise configuration of the drainage system would be determined with the review and approval of each phase of the Specific Plan. All facilities within the TCSP area would be funded, permitted, and maintained by a Master Property Association, Community Facilities District, or other private entity as approved by the City. Implementation of the TSCP and buildout of the nine additional parcels would be required to comply with local, state and federal water quality and discharge requirements.
<i>ICS 13.3. Stormwater Detention Basins. Design stormwater detention basins to ensure public safety, to be either visually attractive or unobtrusive, provide temporary or permanent wildlife habitats, and recreational uses where feasible in light of safety concerns.</i>	<u>Level II - Consistent.</u> The proposed project would include underground drainpipes and detention basins. The detention basins and infiltration areas would be located along the southern portion of the TCSP area, adjacent to Teal Club Road and the Business Research Park. The location would be visually unobtrusive.
<i>ICS 13.4. Low Impact Development. Incorporate low impact development (LID) alternatives for stormwater quality control into development requirements. LID alternatives include: (1) conserving natural areas and reducing imperviousness, (2) runoff storage, (3) hydro-modification (to mimic pre-development runoff volume and flow rate), and (4) public education.</i>	

Table 9-1
2030 General Plan Policy Consistency

General Plan Policy	Discussion
ENVIRONMENTAL RESOURCES	
<i>ER 1.1. Protect Oxnard's Natural and Cultural Resources. Protect the City's natural resource areas, fish and wildlife habitat, scenic areas, open space areas, parks, and cultural and historic resources from unnecessary encroachment or harm and if encroachment or harm is necessary, fully mitigate the impacts to the maximum extent feasible.</i>	<u>Level II - Consistent.</u> No natural resource areas, habitat, historic resources, or other natural or cultural resources are located adjacent to the project area; therefore, no encroachment would occur. Nesting birds and/or monarch butterflies, as well as irrigation ditches that may be biological resources would be mitigated to the maximum extent feasible under mitigation measures as described in Section 4.4, Biological Resources.
<i>ER 1.2. Protect Surrounding Agriculture and Open Space. Protect open space and agricultural uses around Oxnard through continued adherence to the Guidelines for Orderly Development, Ventura County Greenbelt programs, the Save Open-Space and Agricultural Resources Ordinance, and other programs or policies that may subsequently be adopted such as the SB 375 Sustainable Communities Strategy.</i>	<u>Level II - Consistent.</u> Implementation of the TCSP and Annexation of the nine parcels would not encroach upon agricultural areas to the west of the site. Interim agriculture buffers, as described in Mitigation Measure AG-2, would protect agricultural land on the project area during and after development of Phase 1 and before development of Phase 2. See Section 4.2, Agricultural Resources for a more detailed discussion.
<i>ER 2.3. Promote Areas for Open Space. Reserve, preserve, and promote areas particularly suited for open space/recreational uses. Appropriate public access to these resources shall be preserved, enhanced, restored, and properly controlled.</i>	<u>Level I - Consistent.</u> Within the residential and commercial PA's, a 6.5-acre community park, a 3.5-acre community park, and a 0.4 acre greenbelt combine for a total of 10.4 park acres. A 7.4-acre City Community Park would also be included in the TCSP to the east of Patterson Road. Public access to the Community Park would be available to all Oxnard residents via Patterson Road.
<i>ER 3.2. Review of Development Proposals. Review development proposals in accordance with applicable Federal, State, and local statutes protecting special-status species and jurisdictional wetlands and be open to requiring greater protection.</i>	<u>Level II - Consistent.</u> As discussed in Section 4.4, Biological Resources, there are no special-status species or known jurisdictional wetlands on the project area. Nesting birds and monarch butterflies would be protected through mitigation measures as described in Section 4.4, Biological Resources.
<i>ER 3.5. Reduce Construction Silt and Sediment. Require that construction-related silt and sediment be minimized or prohibited to minimize temporary impacts on biological resources.</i>	<u>Level II - Consistent.</u> As described in Section 4.8, Hydrology and Water Quality, compliance with NPDES Construction General Permit and the City of Oxnard ordinance requiring implementation of a Stormwater Pollution Control Plan would ensure that impacts on biological resources would be minimized.



Table 9-1
2030 General Plan Policy Consistency

General Plan Policy	Discussion
<i>ER 5.5. Abandoned Water Wells and Transfer of Water Rights. Require immediate capping of abandoned water wells at the time of abandonment and where appropriate and feasible, require and accept transference of water rights to the City.</i>	<u>Level II - Consistent.</u> One well currently exists on the project area. The existing agricultural water rights on the site would be transferred to the City as part of the proposed Annexation under applicable rules of the Fox Canyon Groundwater Management Agency.
<i>ER 5.7. Minimizing Paved Surfaces. Require minimization and/or permeability of paved surfaces in new developments and replacement paving, where feasible.</i>	<u>Level II - Consistent.</u> The TCSP includes greenbelts and parks throughout the TCSP area, as well as detention basins located along the southern boundary of the TCSP area. Impermeable surfaces, including buildings, surface parking lots, and streets do not exceed requirements. See Section 4.8, Hydrology and Water Quality for more details.
<i>ER 6.1. Incorporate Views in New Development. Preserve important public views and viewsheds by ensuring that the scale, bulk and setback of new development does not significantly impede or disrupt them and ensure that important vistas and view corridors are enhanced. Require development to provide physical breaks to allow views into these vistas and view corridors.</i>	<u>Level II - Consistent.</u> As discussed in Section 4.1, Aesthetics impacts to scenic views would be less than significant. Many of the views currently available from in and around the project area would continue to be accessible via internal streets and across the proposed parks and other open areas.
<i>ER 6.6. New Development Private Open Space. Ensure that new development incorporates open space areas that provide community and neighborhood identity, private quality exterior private open space for each housing unit, and minimize conflicting land uses and noise generators.</i>	<u>Level II - Consistent.</u> The TCSP would include 17.8 acres of parks and open space, including a 7.4-acre City Community Park. The Community Park would be located between the TCSP and existing agricultural uses west of Patterson Road, which would minimize land use conflicts between urban uses and agricultural uses, including those related to air quality and noise.
<i>ER 9.4. Human Scale Development. Ensure that all new development emphasizes a human, pedestrian scale and minimizes its effect on the area's sensitive visual resources.</i>	<u>Level II - Consistent.</u> The TCSP would be an "Urban Village," emphasizing mixed land uses (single- and multi-family residential, retail, commercial, a business research park, and public/semi-public uses), pedestrian orientation and scale, transit accessibility, and neighborhood identity. As described in Section 4.1, <i>Aesthetics</i> , development within the TCSP would be subject to the <u>Oxnard Design Review Process & Guidelines, which include guidelines related to compatibility with existing development and reducing effects on sensitive visual resources.</u>

Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>ER 10.1. Promote use of Native and Water Wise Plants. Promote the development of a native, drought-tolerant landscape character throughout the City that re-enforces a unified and cohesive landscape character and discourage plants that are invasive or problematic in other ways as determined by the City's landscape architect.</i>	<u>Level II - Consistent.</u> All development within the TCSP area and the nine Annexed parcels would be required to adhere to OCC Chapter 22 Water, Section 22-243 Compliance Requirements, which requires that the landscape area of projects proposing commercial or industrial uses shall be designed without the use of turf and with 100% water wise plants. The landscape area of single-family residential, multi-family residential projects shall be designed with no more than 40% of the landscaped area in turf or plants that are not water wise plants.
<i>ER 11.1. Archaeological Resource Surveys. Continue to require a qualified archaeologist to perform a cultural resources study prior to project approval. Inspection for surface evidence of archaeological deposits, and archaeological monitoring during grading should be required in areas where significant cultural resources have been identified or are expected to occur.</i>	<u>Level II - Consistent.</u> Ground disturbance that has occurred on the project area during past development and agricultural activities, as well as the lack of natural surface water features, reduces the likelihood that intact prehistoric cultural resources are present. However, mitigation measures CR-1(a) through CR-1(c) would minimize impacts to cultural resources by requiring monitoring during grading (all earth disturbing work within the vicinity of the find would be temporarily suspended or redirected until an archaeologist has evaluated) and procedures for discovery of unearthened cultural resources.
<i>ER 11.6. Identification of Archaeological Resources. In the event that archaeological/paleontological resources are discovered during site excavation, continue to require that grading and construction work on the project site is suspended until the significance of the features can be determined by a qualified archaeologist/paleontologist.</i>	
<i>ER 12.5. Soil Conservation and Transfer. Encourage the conservation of agricultural soils by requiring, if feasible and warranted by expert opinion, the transfer of topsoil from agricultural land being developed for urban uses.</i>	<u>Level II - Consistent.</u> As described in Section 4.2, Agricultural Resources, the conversion of agricultural land and its soils to non-agricultural uses would be a significant and unmitigable impact. Transfer of soils would result in potentially significant air quality and noise impacts related to excavation and hauling; in addition, no especially suited or needed receiver site has been identified. Buildout of the TCSP would be primarily at-grade and would not require substantial hauling of soil for disposal.



Table 9-1
2030 General Plan Policy Consistency

General Plan Policy	Discussion
<i>ER 12.11. Urban / Agricultural Buffer Zones. Ensure adequate buffers between residential and agricultural uses, such as open space, recreational facilities, utility easements, windrows, and parking areas. Adequate fencing should be provided around agricultural areas to prevent vandalism.</i>	<u>Level II - Consistent.</u> As described in Section 2.0, Project Description, the TCSP would include buffer zones. A 7.4-acre public park would be located east of Patterson Road between the TCSP and agricultural uses. Interim buffers, as required by Mitigation Measure AG-2, would reduce conflicts between proposed residential uses and existing agricultural uses within the TCSP area.
<i>ER 14.1. Incorporate Ventura County AQMP Mitigations. Incorporate construction and operation mitigation measures recommended or required by the current Ventura County Air Quality Management Plan (AQMP) when preparing CEQA reviews, as appropriate.</i>	Level II - Consistent. As described in Section 4.3, Air Quality, mitigation measures consistent with this policy would be required for development within the TCSP and nine parcels proposed for Annexation.
<i>ER 14.2. Transportation Demand Management (TDM). Employ best traffic management practices such as bus turnouts and traffic signal synchronization in order to reduce traffic-related air emissions impacts; require commercial developers to improve public transit service between residential and employment uses or shopping centers, bike lanes and protected bicycle parking areas, and other project features that would reduce the need for automobile trips related to the development; and require Transportation Management Associations (TMA) for projects that may have adverse air quality impacts related to mobile sources and contributions to off-site TDM funds to reduce residual impacts that cannot be mitigated on a project-specific basis.</i>	<u>Level II - Consistent.</u> The transit oriented development of the Urban Village would reduce air quality impacts by creating a community that would be accessed by pedestrians and would be in close proximity to alternative modes of transportation, such as buses. Enhanced bus facilities on Ventura Road are included in the proposed plan. This would include multiple bus pull-out locations and bus shelters on southbound and northbound lanes of Ventura Road, which would be built as part of the project. The planned bus stops would serve the project area and would help provide public transit options. See Section 4.3, Air Quality for Mitigation Measure AQ-2(a) which requires payment of fees to a TDM, as well as Section 4.13, Transportation and Traffic, which includes mitigation measures to improve intersection and roadway operations, reducing traffic and, therefore, emissions, at intersections.
<i>ER 14.3. Reducing Carbon Monoxide Exposure at Congested Intersections. Require mitigation measures that consider prohibiting the construction of residences or buildings lacking ventilation systems at congested intersections with the potential for excessive Carbon Monoxide “hot spot” exposure to sensitive receptors.</i>	<u>Level II - Consistent.</u> See Section 4.3, Air Quality for discussion of Carbon Monoxide “hot spot” risks. It was determined that future traffic combined with project traffic would not cause an exceedance of either the state or federal CO standards in 2020 or 2025 and project-related CO impacts would be less than significant.

Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>ER 14.4. Emission Control Devices. Require all construction equipment to be maintained and tuned to meet appropriate EPA, CARB, and VCAPCD emissions requirements and when new emission control devices or operational modifications are found to be effective, such devices or operational modifications are required on construction equipment.</i>	<u>Level II - Consistent.</u> See Section 4.3, Air Quality for requirements related to construction equipment, which ensure consistency with this policy.
<i>ER 14.5. Reducing Construction Impacts during Smog Season. Require that the construction period be lengthened to minimize the number of vehicles and equipment operating at the same time during smog season (May through October).</i>	<u>Level II - Consistent.</u> See Section 4.3, Air Quality, which includes the requirement that the construction period be lengthened in Mitigation Measure AQ-1(b).
<i>ER 14.6. Minimizing Dust and Air Emissions through Permitting Requirements. Continue to require mitigation measures as a condition of obtaining building or use permits to minimize dust and air emissions impacts from construction.</i>	<u>Level II - Consistent.</u> Mitigation measures consistent with this policy are included in the EIR in Section 4.3, Air Quality, mitigation measures AQ-1(a and b), which require dust control measures and construction equipment controls in accordance with VCAPCD requirements.
<i>ER 14.7. Mitigation Monitoring. Ensure that projects with identified air quality impacts in their respective EIRs are subject to effective mitigation monitoring as required by AB 3180.</i>	<u>Level II - Consistent.</u> The Mitigation Monitoring Program includes specific details on how each mitigation measure is monitored, including those related to air emissions in compliance with CEQA Guidelines 7.15097.



Table 9-1
2030 General Plan Policy Consistency

General Plan Policy	Discussion
<i>ER 14.12. Use VCAPCD Air Quality Assessment Guidelines. Use the VCAPCD Air Quality Assessment Guidelines and recommended analytical tools for determining and mitigating project air quality impacts and related thresholds of significance for use in environmental documents. The City shall continue to cooperate with the VCAPCD in the review of development proposals.</i>	<u>Level II - Consistent.</u> See Section 4.3, Air Quality for discussion of the TCSP's consistency with the VCAPCD Air Quality Assessment Guidelines. This EIR determined that operational emissions related to air quality would be significant and unmitigable based on these guidelines. All project developers would be required to use the guidelines for specific project development within the TCSP and nine parcels proposed for annexation.

SAFETY & HAZARDS

<i>SH 1.1. Minimize Liquefaction Risk. Ensure that structures for human occupancy are only constructed or placed on a potential liquefaction site if the approved geological report shows that an acceptable hazard risk would be created and/or required mitigation measures are met.</i>	<u>Level II - Consistent.</u> Mitigation consistent with this policy is included in Section 4.5, Geology and Soils. Impact GEO-2 discusses liquefaction and Mitigation Measure GEO-2 would ensure that liquefaction risk would be minimized.
<i>SH 1.2. Minimize Subsidence Trends. Avoid increases in the level of groundwater extraction as a method for meeting new water demands if the extraction leads to subsidence, or unless a comprehensive reinjection program is approved and implemented to offset extractions.</i>	<u>Level II - Consistent.</u> Mitigation consistent with this policy is included in Section 4.5, Geology and Soils. Mitigation Measure GEO-2 would ensure that subsidence risk would be minimized. Furthermore, the proposed project would be water neutral and would not require increased groundwater extraction to meet new water demands.
<i>SH 1.8. Mitigating Seismic Hazards. Where necessary, utilize the expert mitigation measures such as those identified in Special publication 117: Guidelines for Analyzing and Mitigating Seismic Hazards in California (prepared by the Southern California Earthquake Center) to minimize risk associated with seismic activity.</i>	<u>Level II - Consistent.</u> As discussed in Section 4.5, Geology and Soils, mandatory compliance with applicable City of Oxnard and California Building Code requirements would mitigate seismic hazards to a less than significant level.

Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>SH 5.4. Older Neighborhood Noise Mitigation. Develop a noise research and mitigation program for any area where traffic generated noise is significant and exceeds or is likely to exceed acceptable thresholds.</i>	<u>Level II - Consistent.</u> As described in Section 4.10, Noise, impacts would be less than significant other than potential traffic noise impacts on future development within the proposed project area. Mitigation Measure N-4(a) would require future applicants for development within noise contours that would exceed City standards to retain a professional acoustical consultant to conduct an acoustical analysis.
<i>SH 5.6. Compatibility with Oxnard Airport. Work with the Oxnard Airport in revising flight paths to minimize flyovers of residential areas, especially "touch and go" pattern flying at low altitude and at relatively high frequency.</i>	<u>Level II - Consistent.</u> Prior to making a decision on the proposed project, the City of Oxnard must refer the proposed project to the ALUC for review and comment. The ALUC would then review the project for consistency with the Airport Comprehensive Land Use Plan, including the policies and standards discussed above. The City must consider the comments of the ALUC prior to making a decision on adoption of the Specific Plan. Mitigation measures HAZ-5(a-c) would also ensure consistency with airport safety through requirements such as limiting the density of new land uses.
<i>SH 6.1. Construction Noise Control. Provide best practices guidelines to developers for reducing potential noise impacts on surrounding land uses.</i>	<u>Level II - Consistent.</u> As described in Section 4.10, Noise, impacts would be less than significant for surrounding land uses.
<i>SH 6.2. Limiting Construction Activities. Continue to limit construction activities to the hours of 7 am to 7 pm, Monday through Saturday. No construction shall occur after hours, on Sundays, or national holidays without permission from the City.</i>	<u>Level II - Consistent.</u> As described in Section 4.10, Noise, construction hours would be limited to 7am to 6pm Monday through Saturday.
<i>SH 6.3. Buffering of Sensitive Receptors. Require noise buffering and/or other construction treatments in development located near major streets, highways, the airport, rail road tracks, or other significant noise sources as recommended by a noise analysis.</i>	<u>Level II - Consistent.</u> As described in Section 4.10, Noise, impacts would be less than significant other than potential traffic noise impacts on future development within the proposed project area. Mitigation Measure N-4(a) would require future applicants for development within noise contours that would exceed City standards to retain a professional acoustical consultant to conduct an acoustical analysis.



Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>SH 6.4. New Development Noise Compatibility. Require that proposed development projects not generate more noise than that classified as "satisfactory" based on CEQA Thresholds of significance on nearby property.</i>	<u>Level II - Consistent.</u> As described in Section 4.10, Noise, impacts would be less than significant and the proposed project would not generate noise that exceeds significant thresholds on any nearby properties.
<i>SH 6.5. Land Use Compatibility with Noise. Encourage non-noise sensitive land uses to locate in areas that are permanently committed to noise producing land uses, such as transportation corridors and industrial zones.</i>	<u>Level II - Consistent.</u> The additional nine parcels proposed for Annexation south of Teal Club Road are designated for Airport Compatible land uses, as shown on the Oxnard 2030 General Plan land use map and would be developed with light industrial uses, which are compatible with the noise levels generated by the Oxnard Airport. As discussed in Impact LU-3, industrial development would not impact nearby residences.
<i>SH 6.7. Peak Noise Evaluation Along Truck Routes. Evaluate peak event noise impacts for existing and proposed development along existing or proposed designated truck routes and require feasible and appropriate mitigations for project subject to discretionary review and approval.</i>	<u>Level II - Consistent.</u> As described in Section 4.10, Noise, impacts would be less than significant other than potential traffic noise impacts on future development within the proposed project area. No truck routes are located adjacent to the project area. However, Mitigation Measure N-4(a) would require future applicants for development within noise contours that would exceed City standards to retain a professional acoustical consultant to conduct an acoustical analysis.
<i>SH 6.9. Minimize Noise Exposure to Sensitive Receptors. Prohibit the development of new commercial, industrial, or other noise generating land uses adjacent to existing residential uses, and other sensitive noise receptors such as schools, child and daycare facilities, health care facilities, libraries, and churches if noise levels are expected to exceed 70 dBA.</i>	<u>Level II - Consistent.</u> As described in Section 4.10, Noise, impacts would be less than significant other than potential traffic noise impacts on future development within the proposed project area. Mitigation Measure N-4(a) would require future applicants for development within noise contours that would exceed City standards to retain a professional acoustical consultant to conduct an acoustical analysis. No residential uses or other sensitive noise receptors such as schools, child and daycare facilities, health care facilities, libraries, or churches would be exposed to noise levels exceeding 70 dBA as a result of the proposed project.

Table 9-1
2030 General Plan Policy Consistency

<i>General Plan Policy</i>	<i>Discussion</i>
<i>SH 6.12. Development Near Railroads and Oxnard Airport. Require that new habitable structures be setback at least 85 feet from the nearest railroad track measured from the edge of the outermost railroad track, and only compatible new development is located within the Oxnard Airport 65 dBA CNEL contour.</i>	<u>Level II - Consistent.</u> Prior to making a final decision on the TCSP project, the City of Oxnard will refer it to the ALUC for a consistency review with the Airport Land Use Plan. Only a small portion of the TCSP area is within the 65 dBA CNEL contour and it would not contain residences or other incompatible land uses. The nine parcels proposed for Annexation consist of land that is within the 65 dBA CNEL contour; however, this land would be zoned for light industrial uses, which are compatible. Mitigation measures HAZ-5(a-c) would also ensure consistency with airport safety and noise regulations.
<i>SH 6.13. Noise Acceptable for Open Windows and Patios. Continue to require noise analysis of proposed development projects as part of the environmental review process and the require mitigation measures to reduce noise impacts to acceptable levels within outside activity areas and within residential structures without relying on mechanical ventilation, if feasible.</i>	<u>Level II - Consistent.</u> As described in Section 4.10, Noise, impacts would be less than significant other than potential traffic noise impacts on future development within the proposed project area. Mitigation Measure N-4(a) would require future applicants for development within noise contours that would exceed City standards to retain a professional acoustical consultant to conduct an acoustical analysis.
<i>SH-7.12 Hazardous Materials Studies. Ensure that the proponents of new development projects address hazardous materials concerns through the preparation of phase 1 or phase 2 hazardous materials studies for each identified site as part of the design phase for each project. Recommendations required to satisfy federal or State cleanup standards outlined in the studies will be implemented as part of the construction phase for each project.</i>	<u>Level II - Consistent.</u> Mitigation Measure HAZ-3 requires adherence to recommendations made in the Phase 1 for the TCSP and nine parcels proposed for Annexation. This would require removal and recompaction of soil, as well as continued monitoring and appropriate remediation of potentially contaminated soil.



Table 9-1
2030 General Plan Policy Consistency

General Plan Policy	Discussion
<i>SH 9.1. Airport Land Use Compatibility Plans. Require development around the Oxnard and Camarillo Airports to be consistent with the safety policies and land use compatibility guidelines contained within the Ventura County Airport Land Use plan.</i>	<u>Level II - Consistent.</u> The additional nine parcels proposed for Annexation south of Teal Club Road are designated for Airport Compatible land uses, as shown on the Oxnard 2030 General Plan land use map. Upon Annexation, the additional nine parcels proposed for annexation south of Teal Club Road would be rezoned Light Manufacturing (M-1) by the City of Oxnard and would be compatible. Mitigation measures HAZ-5(a-c) would also ensure consistency with airport safety and noise regulations for the entire project area.
<i>SH 9.2. Compliance with FAA Regulations. Ensure development within the airport approach and departure zones are in compliance with applicable Federal Aviation Administration regulations that address objects affecting navigable airspace.</i>	<u>Level II - Consistent.</u> The additional nine parcels proposed for Annexation south of Teal Club Road are designated for Airport Compatible land uses, as shown on the Oxnard 2030 General Plan land use map. Upon Annexation, the additional nine parcels proposed for annexation south of Teal Club Road would be rezoned Light Manufacturing (M-1) by the City of Oxnard. Development on the parcels would be reviewed to ensure compliance with the FAA regulations at the time such development is proposed. Mitigation measures HAZ-5(a-c) would also ensure consistency with airport safety and noise regulations for the entire project area through requirements including notification of the FAA.

HOUSING

<i>Housing Element Policy-2.2 Balanced Opportunities. Provide opportunities to the private and public sector for the production of housing that meets the needs of special needs-, extremely low-, very low-, low-, moderate, and above moderate-income housing to achieve a balanced community.</i>	<u>Level II - Consistent.</u> The TCSP envisions development of up to 990 residential dwelling units in a variety of densities and product types including both market-rate and affordable housing (15% affordable). In addition to single-family residential units, the TCSP includes single-family courtyard homes, single-family townhomes and multi-family condominiums and apartments (Section 2.0, Project Description).
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APPENDIX 1

LANDSCAPE PALETTE

The objective of the landscaping palette is to provide a clear definition and identity to the Teal club Specific Plan area. The primary perimeter streets and the internal collector streets are intended to have landscape palettes that encourage pedestrian and cyclist activity in a safe, visible and aesthetically pleasing environment. The landscape within the individual neighborhoods of the Specific Plan shall be designed with the goal of creating individual identity and visual quality consistent with the quality of architecture and building materials within the neighborhood.

The attached list of plant materials is included as an Appendix in order to provide flexibility in modification by the City of Oxnard departments responsible for selection of plant materials. As best practices and governmental policies related to landscape planting and maintenance are periodically revised, the Appendix Plant Palette may also be revised as approved by the Director of Community Development.

The list of plant materials specified herein is intended to serve as a guide during preparation of formal landscaping plans. While exact plant species is not required, the use of plant species not listed in the appendix must be proven to have comparable drought tolerances and aesthetic quality, and must not be invasive. Locations and acceptable sizes are listed for significant locations. Where a location is not listed, similar planting palettes shall be required.

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Teal Club Specific Plan

Street Trees		
Ventura Road 36" box size Median 24" box size (minimum)	Cinnamomum camphora (new) Pinus canariensis	Camphor Canary Island Pine
Patterson Road 36" box size <i>Median</i> 24" box size (minimum)	Platanus x 'Bloodgood' (match existing) Pinus canariensis Tipuanua tipu	London Plane Canary Island Pine Tipu Tree
Doris Avenue 36" box size <i>Median</i> 24" box size (minimum)	Cupaniopsis anacardioides (match existing) Pinus canariensis Magnolia g. 'St. Mary'	Carrot Wood Canary Island Pine Southern Magnolia
Teal Club Road 36" box size no median in Teal Club Road	Cinnamomum camphora (new)	Camphor

Street Trees (cont.)		
Beverly Drive 36" box size	Cinnamomum camphora	Camphor
Entry at Ventura Road	Metrocideros excelsa	New Zealand Christmas Tree
	Washingtonia robusta	Mexican Fan Palm-matching heights
Coronado Place 36" box size	Koelreuteria paniculata	Goldenrain Tree
Round-About 24" box size (minimum)	Olea europaea 'Swan Hill'	Fruitless Olive
	Metrocideros excelsa	New Zealand Christmas Tree
	Washingtonia robusta	Mexican Fan Palm-matching heights
Retail and Commercial Mixed-Use		
Primary Trees <i>Dominant trees - minimum 70% of total trees</i> 24" box minimum size	Cinnamomum camphora Koelreuteria paniculata Pinus canariensis Pistacia chinensis Quercus wislizenii Ulmus parvifolia 'Drake'	Camphor Tree Goldenrain Tree Canary Island Pine Chinese Pistache Interior Live Oak Evergreen Elm
Secondary Trees <i>Background trees</i> 24" box minimum size	Cupaniopsis anacardioides Liquidamber s. 'Festival' Pinus eldarica Podocarpus gracilior Prunus caroliniana Pyrus calleryana	Carrot Wood American Sweet Gum Afghan Pine Fern Pine Carolina Laurel Cherry Evergreen Pear
Accent Trees <i>At visual focal areas</i> 24" box minimum size	Cercis occidentalis Lagerstroenia indica Olea europaea "Swan Hill" Pyrus calleryana	Western Redbud Crape Myrtle Fruitless Olive Flowering Pear

Teal Club Specific Plan

Single Family Residential		
Primary Trees <i>Dominant tree - minimum</i> <i>70% of total</i> <i>24" box minimum size</i>	Arbutus 'Marina' Celtis sinensis Cinnaomoum camphora Koelreuteria paniculata Pinus canariensis Pinus pinea Pistacia chinensis Platanus a. 'Bloodgood' Rhus lancea Ulmus parvifolia 'Drake'	Strawberry Tree Chinese Hackberry Camphor Tree Goldenrain Tree Canary Island Pine Italian Stone Pine Chinese Pistache London Plane Tree African Sumac Evergreen Elm
Secondary Trees <i>Background trees</i> <i>24" box minimum</i>	Fraxinus o. 'Raywood' Liquidamber s. 'Festival' Liriodendron tulipifera Magnolia grandiflora Pinus eldarica Pinus halepensis Prunus caroliniana Pyrus calleryana Sequoia s. 'Soquel'	Raywood Ash American Sweet Gum Tulip Tree Southern Magnolia Afghan Pine Aleppo Pine Carolina Laurel Cherry Flowering Pear Coast Redwood
Accent Trees <i>At visual focal areas</i> <i>24" box minimum</i>	Cercis occidentalis Lagerstroenia indica Olea europaea 'Swan Hill' Prunus c. 'Krauter Vesuvius' Pyrus calleryana	Western Redbud Crape Myrtle Fruitless Olive Purple-Leaf Plum Flowering Pear

Business Research Park		
Primary Trees <i>Dominant tree - minimum 70% of total 24" box minimum size</i>	Arbutus 'Marina' Cedrus deodara Cinnaomoum camphora Koelreuteria paniculata Pinus eldarica Pistacia chinensis Rhus lancea Ulmus parvifolia 'Drake'	Strawberry Tree Deodar Cedar Camphor Tree Goldenrain Tree Afghan Pine Chinese Pistache African Sumac Evergreen Elm
Secondary Street Trees <i>Background trees 24" box minimum size</i>	Cupaniopsis anacardioides Fraxinus o. 'Raywood' Liquidamber s. 'Festival' Liriodendron tulipifera Pinus halepensis Podocarpus gracilior Prunus caroliniana Pyrus calleryana	Carrot Wood Raywood Ash American Sweet Gum Tulip Tree Aleppo Pine Fern Pine Carolina Laurel Cherry Flowering Pear
Accent Trees <i>At visual focal areas 24" box minimum size</i>	Cercis occidentalis Lagerstroenia indica Olea europaea 'Swan Hill' Pyrus calleryana	Western Redbud Crape Myrtle Fruitless Olive Flowering Pear

Parks, Greenbelts and Open Space		
<p>Trees</p> <p><i>70% 24" box</i></p> <p><i>30% 15 gallon</i></p>	<p>Arbutus 'Marina'</p> <p>Cedrus deodara</p> <p>Cercis occidentalis</p> <p>Cinnamomum camphora</p> <p>Koelreuteria paniculata</p> <p>Lagerstroemia indica</p> <p>Liquidambar s. 'Festival'</p> <p>Liriodendron tulipifera</p> <p>Pinus canariensis</p> <p>Pinus eldarica</p> <p>Pinus pinea</p> <p>Pistacia chinensis</p> <p>Platanus x a. 'Bloodgood'</p> <p>Platanus racemosa</p> <p>Prunus caroliniana</p> <p>Pyrus calleryana</p> <p>Rhus lancea</p> <p>Ulmus parvifolia 'Drake'</p>	<p>Strawberry Tree</p> <p>Deodar Cedar</p> <p>Western Redbud</p> <p>Camphor Tree</p> <p>Goldenrain Tree</p> <p>Crape Myrtle</p> <p>American Sweet Gum</p> <p>Tulip Tree</p> <p>Canary Island Pine</p> <p>Afghan Pine</p> <p>Italian Stone Pine</p> <p>Chinese Pistache</p> <p>London Plane Tree</p> <p>Western Sycamore</p> <p>Carolina Laurel Cherry</p> <p>Evergreen Pear</p> <p>African Sumac</p> <p>Evergreen Elm</p>
Shrubs - All Developed Areas		
<p>Large Shrubs</p> <p><i>5' + tall</i></p> <p><i>1 gallon minimum size</i></p>	<p>Abelia "Edward Goucher"</p> <p>Baccharis sp.</p> <p>Calyccanthus occidentalis</p> <p>Camellia japonica</p> <p>Ceanothus species</p> <p>Cotoneaster lacteus</p> <p>Dodonaea viscosa</p> <p>Escallonia fradesii</p> <p>Fiejoa sellowiana</p> <p>Ilex cornuta</p> <p>Juniperus chinensis</p>	<p>Glossy Abelia</p> <p>Coyote Bush</p> <p>Spice Bush</p> <p>Japanese Camellia</p> <p>California Lilac</p> <p>Cotoneaster</p> <p>Hopseed Bush</p> <p>Escallonia</p> <p>Pineapple Guava</p> <p>Chinese Holly</p> <p>Juniper</p>

Shrubs - All Developed Areas (cont.)		
Large Shrubs <i>5' + tall</i> <i>1 gallon minimum size</i>	Ligustrum texanum Nerium oleander Photinia fraserii Pittosporum tobira Podocarpus macrophyllus Prunus laurocerasus Pyracantha 'Santa Cruz' Rhamnis californica Rhamnus ilicifolia Viburnum "Spring Bouquet" Xylosma congestum	Texas Privet Oleander Red-tip Photinia Mock Orange Yew Pine English Laurel Fire Thorn Coffeeberry Hollyleaf Redberry Viburnum Shiny Xylosma
Medium Shrubs <i>3' – 5' tall</i> <i>1 gallon minimum size</i>	Agave species Azalea species Berberis thunbergii Bougainvillea species Buxus japonica Callistemon 'Little John' Camellia sasanqua Ceanothus sp. Cistus sp. Dietes bicolor Grevillia noelii Nandina domestica Nerium oleander (dwarf var.) Pennisetum rubrum Raphiolepis indica sp. Rose sp. Salvia species	Agave Southern Azalea Japanese Barberry Bougainvillea Boxwood Dwarf Bottlebrush Sasanqua Camellia California Lilac Rock Rose Butterfly Iris Grevillea Heavenly bamboo Dwarf Oleander Red Fountain Grass India Hawthorn Rose Sage
Small Shrubs <i>1' – 3' tall</i> <i>1 gallon minimum size</i>	Agapanthus africanus Artemesia sp. Baccharis "Pigeon Point" Ceanothus sp. Cotoneaster dammeri Hemerocallis hybrid Heuchera sanguinea Juniperus horizontalis Lantana montevidensis Lavendula spp. Liriope gigantea	Lily of the Nile Artemesia Dwarf Coyote Bush Dwarf Ceanothus Bearberry Cotoneaster Evergreen Daylily Coral Bells Juniper Lantana Lavender Liriope

Teal Club Specific Plan

Shrubs (cont.)		
Small Shrubs <i>1' – 3' tall</i> <i>1 gallon minimum size</i>	Lupinus albifrons Mahonia aquifolium “Compacta” Nandina “Harbor Dwarf” Pittosporum t. “Wheeler’s Dwarf” Raphiolepis i. “Clara” Rosemarinus prostratus Salvia gregii Santolina rosmarinifolia Zauschneria canum	Bush Lupine Dwarf Oregon Grape Dwarf Heavenly Bamboo Dwarf Mock Orange India Hawthorn Dwarf Rosemary Salvia Santolina California Fuchsia
Groundcover and Perennials		
<i>1 gallon minimum size or flatted rooted cuttings</i>	Arctostaphylos sp. Baccharis “Twin Peaks” Coprosma “Verde Vista” Gazania sp. Hypericum calycinum Iris douglasiana Juniperus confeta Myoporum pacificum Trachelospermum jasminoides	Dwarf Mazanita Dwarf Coyote Brush Coprosma Gazania St. Johns Wort California Iris Shore Juniper Myoporum Star Jasmine
Vines		
<i>1 gallon minimum size</i>	Campsis radicans Clytostoma sp. Ficus pumila Lonicera japonica Parthenocissus quinquefolia Parthenocissus tricuspidata Rosa sp. Wisteria sinensis	Blood-Red Trumpet Vine Violet Trumpet Vine Creeping Fig Japanese Honeysuckle Virginia Creeper Boston Ivy Climbing Rose Wisteria
Turf		
<i>Seeded or sodded – no turf areas less than 8' wide</i>	Water conserving tall-type fescue mix	

Specialty Landscape Areas: Vegetated Drainage Areas:

Vegetated swales slowly convey runoff flow to downstream discharge points and will be planted with species adapted to seasonal inundation and extended periods of dry conditions. The following plant list does not include proposed trees and shrubs for the perched areas in the swales as illustrated in the sections.

Water Quality Basins <i>15 gal. min.</i>	Aesculus californica Heteromeles arbutifolia Juglans californica Populus fremontii Platanus racemosa Prunus ilicifolia Quercus agrifolia Quercus berberidifolia Quercus wislizenii	California Buckeye Toyon Black Walnut Fremont Cottonwood Western Sycamore Holly-leaved Cherry California Live Oak Interior Scrub Oak Interior Live Oak
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